

**CITY OF TEMPE
DEVELOPMENT REVIEW COMMISSION**

Meeting Date: 05/24/2016
Agenda Item: 6

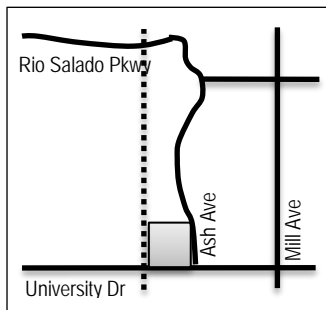
ACTION: Request for an Amended Planned Area Development Overlay and Development Plan Review consisting of a new mixed-use development with a grocery store and residences and a Use Permit for tandem parking for THE FOUNDRY, located at 204 West University Drive. The applicant is Darin Sender of Sender and Associates.

FISCAL IMPACT: While this ordinance change does not directly impact revenue, the planned development will result in collection of the standard development fees, calculated according to the approved fee structure at the time of permit issuance.

RECOMMENDATION: Staff – Approval, subject to conditions

BACKGROUND INFORMATION: THE FOUNDRY (PL160099) is on a vacant lot formerly used as a grocery cooperative. The property was rezoned from General Industrial District to City Center District in 2005 with a Planned Area Development Overlay for a new grocery and multi-family development. An amendment to that PAD was made in 2006 and a use permit for tandem parking was granted in 2007; however the development was never built. The proposed project is similar in concept to the earlier entitlement, with a 41,000 s.f. grocery store and approximately 292 residential units. The request includes:

1. Amended Planned Area Development Overlay for a maximum of 130' building height, minimum of 0' setbacks, a maximum of 161 dwelling units per acre density, a range of units between 260 and 300 units and parking by a downtown shared model for a mixed use development including a 41,000 s.f. grocery store with residential apartments.
2. Use Permit to allow 38 tandem parking spaces for residential parking in the garage.
3. Development Plan Review including site plan, building elevations, and landscape plan



Existing Property Owner	University and Ash Owner, LLC
Applicant	Darin Sender, Sender Associates, Chtd.
Zoning District	CC City Center, PAD Planned Area Development
Gross / Net site area	1.858 acres
Density / Number of Units	157 du/ac / 292 units (density range 139-162 du/ac and unit range 260-300 dwelling units)
# of Bedrooms	323 bedrooms
Unit Types	266 one bedroom 21 two bedroom 5 three bedroom
Ground Building Area	74,560 s.f.
Lot Coverage	92% (70% in existing PAD)
Total Building Area	572,278 s.f.
Building Height	118' (up to 130' maximum) (247' maximum in existing PAD)
Building Setbacks	0' front (south), 0' west side, 0' east street side, 0' rear (north) (5', 20', 5', 2' existing PAD, 0' setbacks in CC)
Landscape area	1,619 sf 2% (1,008 sf, 1.2% existing PAD)
Vehicle Parking	557 spaces provided (230 min. required) (446 standard spaces, 73 compact spaces & 38 tandem spaces)
Bicycle Parking	312 spaces provided (287 min. required)

ATTACHMENTS: Development Project File

STAFF CONTACT(S): Diana Kaminski, Senior Planner (480) 858-2391

Department Director: Dave Nakagawara, Community Development Director

Legal review by: N/A

Prepared by: Diana Kaminski, Senior Planner

COMMENTS:

This site is located on the North West corner of University Drive and Ash Avenue, functioning as the transitional cornerstone from Downtown Tempe to the neighborhoods to the west. Chase parking garage is to the north of the site, CenterPoint commercial development is on the east side of Ash Avenue, Union Pacific Railroad is to the west of the site, commercial uses front University Drive to the south, with residences further south. The site was originally zoned General Industrial District and was formerly used for restaurant and grocery uses. The property was rezoned in 2005 to be within the City Center zoning district, and a Planned Area Development was approved to accommodate a mixed-use development consisting of a 53,000 s.f. grocery store, structured parking and 187 apartments within a 247-foot tall building. In 2007 a use permit for tandem parking was granted. The proposed development would amend the existing Planned Area Development to accommodate a mixed-use development consisting of a 41,000 s.f. grocery store, structured parking and 292 apartments within a 118-foot tall building. The site is designated as part of the Urban Center in the Downtown Tempe Design Guidelines, which suggests up to 300 feet of building height east of the railroad tracks, and steps down to 50 feet west of the railroad tracks to transition to the residential areas further west.

This request includes the following:

1. Planned Area Development Amendment
2. Use Permit for 19 tandem parking spaces
3. Development Plan Review which includes: site plan, landscape plan and elevations for a nine-story building with a grocery store with second-floor mezzanine restaurant, three levels of structured parking and up to 300 residential apartments within 570,609 s.f. of building area on 1.858 net acres.

The applicants is requesting the Development Review Commission take action on the Use Permit, and provide recommendation to City Council for items one and three listed above.

PRELIMINARY SITE PLAN REVIEW

2/17/2016 First Site Plan Review comments included clarification about parking provided, tandem requested and compact spaces shown, as the compact spaces would not be counted toward the total required parking by code. Images of possible parking garage screen material were provided. Police provided public safety comments regarding the need to block access on the north side (Chase garage property) and west side, as well as balcony design considerations for safety. A street profile for Ash Avenue was provided to address a requested dedicated left turn lane into the development. Comments were provided on the need for four sided architectural detail, need for more landscape material and attention to the western elevation. Standard conditions were provided for early design consideration in the design development phase of the project. The most significant issues were:

- the directional flow of traffic into and out of the site (counter clockwise) and impacts to traffic on both University and Ash – staff recommended a clockwise orientation, entering from University, exiting onto Ash, which would have required flipping the service bay area of the grocery back of house.
- the request to remove a newly installed median to allow left-bound turns across west flowing traffic on University.
- The request for a dedicated left turn lane on Ash Avenue that would require additional right of way, above the planned right of way needed for the proposed street car lane on Ash Avenue.
- The lack of information on how refuse and fire would circulate on site, specifically vertical clearances, turning radii and collection requirements for staging equipment for safe disposal of solid waste from the grocery store and residences.

A meeting was held after the applicant reviewed the comments, to discuss the circulation needs of the site and other questions the applicant had of the comments made by staff. Staff required a second site plan review prior to formal submittal due to the above items in need of design resolution.

3/9/2016 Second Site Plan Review comments included a need for all plans to be coordinated for accurate dimensions and the building footprint. Request for a separate fire and refuse circulation plan was necessary, as details on the plan did not sufficiently address the concerns of the Solid Waste Services or Fire staff. Comments regarding the garage elevation were

made, as the material changed from the first review, and became less architecturally impacting. The proposed elevation was referenced to artwork at an airport, which was created on chain link; a material staff would not support as part of the building façade. Due to the grocery store size changing from the earlier entitlement, this project is no longer required to participate in the Art in Private Development program; therefore any reference to proposed art would need to be integrated into the elevation architecture as a long term building element. Comments were provided regarding elevation details, the location and visibility of large transformers on site, and the need for a pedestrian access on Ash Avenue to the grocery store, to serve transit patrons. Request for a parking management plan for proposed reductions in parking and utilization of the downtown parking ratios. Request for landscape on the west side of the building. Request for clarification on line nomenclature, building footprint details, and dimensions. Direction was given to remove the left turn median cut on University Drive and to remove the dedicated northbound left turn lane on Ash Avenue.

3/30/2016 Third Site Plan Review included analysis of the parking study with required revisions requested, need for fully dimensioned plans, verification of building height, agreement to allow stabilized granite for the required building egress path on west side of building (to minimize the look of a sidewalk that is not to be used except in emergencies for exiting purposes). Comments regarding the refuse collection access and circulation were provided. Concerns about the garage elevations, the material proposed was painted extruded metal, again with undetermined future artwork. The screening material has two distinct issues: for planning, the architectural solution to parking garages is ideally to fully integrate the parking into the architecture so that it does not appear as parking, but part of the building, for building code, the percentage of opening determines whether a project must pay for and maintain expensive electrical ventilation systems or is open enough to not require fans. The percentage of opening is also related to the distance from the property line to an adjacent property, as is the case on the west and north elevations, where proximity to the property line limits the amount of openings for fire protection the building can have. This solution often results in a solid concrete wall. Staff requested that the issues of the Building Code and the design criteria of the Zoning and Development Code be addressed in the architectural elevations, so that there would be no changes to design caused by later value engineering of the project.

5/9/2016 Additional modifications were made to the elevations, materials and landscape to address staff comments.

Applicant response to Site Plan Review comments:

- The traffic circulation on site could not be switched from counter clockwise to clockwise due to the pre-designed determination of the grocery store footprint, parking garage access ramps, apartment leasing lobby, and required loading zone for grocery deliveries. There was applicant concern that switching the orientation would put refuse and service delivery trucks in conflict with residents, guests and customers entering the site while large trucks were backing into the delivery bays or refuse collection stations.
- The proposed left turn lane median cut was removed. Traffic into and out of the University driveway at the west end would be right in and right out only.
- The proposed left turn lane on Ash Avenue was redesigned per Traffic Engineering direction to be a two-way shared left turn, with the southern-most 75-foot stacking area serving south bound left turns, and the northern-most 75-foot stacking area serving north bound left turns into the development at the Ash Avenue entrance drive at the north end.
- Plans were coordinated between disciplines to remove discrepancies, dimensions and data were verified.
- Most staff comments related to the design of the elevations and landscape plans were addressed; the applicant worked closely with staff to meet design expectations.

PUBLIC INPUT

- A neighborhood meeting was required for this request.
- The neighborhood meeting held on Wednesday, April 6, 2016 from 6:00 p.m. to 8:00 p.m. at the Dennis J. Cahill Senior Center at the Boys & Girls Club on 715 W 5th Street.
- See attached summary of meeting provided by the applicant.
- Community Development staff attended the meeting.
- Approximately 30 members of the public attended.
- Public comments included positive response to getting a grocery store back in the downtown area near the neighborhoods, scale of the building being less than the prior building height seemed more appropriate to the area, interest in connectivity to alternative modes of transportation. Concern about market rate apartments (affordability) and higher end reputation of grocery proposed (Whole Foods) leading to gentrification of the area and not providing

for lower working-income residents.

PROJECT ANALYSIS

CHARACTER AREA PLAN

This site is part of the Rio Salado / Downtown / ASU character area that is currently under development. Prior documents created that may involve this site include the 2006 Community Design Principles: Downtown / Mill Avenue District and Vicinity. The documents were accepted by Council but not adopted. The site is located within the Urban Center height area from the design principals, which identifies building heights up to 300 feet.

PLANNED AREA DEVELOPMENT

The original PAD for this site was approved in 2005, the proposed Amendment to the PAD increases density, number of units, landscape area and lot coverage from the initial PAD, and reduces the building height. The new project reduces the size of the grocery by more than 10,000 s.f. and increases the number of residences by 106. The required parking for the proposed PAD amendment has been revised based on the new parking standards in the City Center Zoning District adopted by City Council in 2015. The table below shows a comparison of the development standards for the City Center Zoning District, the previous PAD Overlay and the new proposed amended PAD Overlay:

PAD Overlay				
Standard	CC, City Center	EXISTING PAD	PROPOSED PAD	Change
Residential Density (du/ac)	-NS-	97 du/ac	158 du/ac (PAD range of 139-162 du/ac)	Increase
Number of Dwelling Units	-	187 units	292 units (PAD range 260 to 300 units)	Increase
Building Height (feet) [Exceptions, see Section 4-205(A)]				
Building Height Maximum	50 ft	247 ft.	118 ft. (PAD maximum 130 ft)	Decrease
Building Height Step-Back Required Adjacent to SF or MF District [Section 4-404, Building Height Step-Back]		Yes	Yes	
Maximum Lot Coverage (% of net site area)	-NS-	70%	92%	-
Minimum Landscape Area (% of net site area)	-NS-	1.2%	2%	-
Setbacks (feet) (a) [Exceptions, see Section 4-205(B)]				
Front (south, facing University Dr.) Parking	0 ft 20 ft	0 ft 20 ft	0 ft 20 ft	-
Side (west)	0 ft	0 ft	0	
Rear (north)		0 ft	0	
Street Side (east, facing Ash Ave.) Parking	0 ft 20 ft	0 ft 20 ft	0 ft 20 ft	
Vehicle Parking		541 required 646 provided	230 required 557 provided (446 standard spaces, 73 compact spaces and 38 tandem spaces)	-
Bicycle Parking (Office & Call Center)		195 required 262 provided	287 required 312 provided	-

PARKING

The table below summarizes the required and proposed vehicle parking for the project, in comparison to the new City Center Parking standards and the proposed parking. The proposed parking meets the new City Center parking standards; the proposed parking is not a change in the PAD. According to the parking management plan, levels one and two will be designated as reserved for commercial uses, inclusive of guest parking, regardless of accessibility or associated fees for parking. Levels three and four will be reserved for residential parking and restricted from public use. With this plan, Level One has 17 standard spaces and two ADA accessible spaces and Level Two has 142 standard spaces, 23 compact and 3 ADA spaces, there are no tandem spaces allocated within the public levels of parking. Level Three has 23 spaces reserved for the grocery. Compact spaces are not defined or recognized within the zoning code, and cannot be accounted for as required parking; however, these 8' x 16' spaces are in excess of required parking, and are being conditioned to be striped and signed as compact only for management of the spaces. 73 compact spaces were shown on the conceptual floorplans, however flexibility was requested to accommodate changes that occur during construction document development; a condition limiting the maximum number of compact spaces to 85 has been included.

Unit Type	Unit Quantity / Square Footage	New CC Parking Ratio & Requirements per ZDC	Proposed Parking
1 bedroom	266 units	.5 spaces per bedroom = 133 spaces	.5 spaces per bedroom = 133 spaces
2 bedroom	22 units	.5 spaces per bedroom = 22 spaces	.5 spaces per bedroom = 22 spaces
3 bedroom	5 units	.3 spaces per bedroom = 4.5 spaces	.3 spaces per bedroom = 4.5 spaces
Guest	292 total units		.1 per unit = 29.3 spaces
Grocery	41,010 s.f.	Subtract 5,000s.f. 1 space per 500s.f. 41010 s.f. – 5,000 s.f.= 36,010 s.f. / 500 = 72 spaces required	Subtract 5,000s.f. 1 space per 500s.f. 41010 s.f. – 5,000 s.f.= 36,010 s.f. / 500 = 72 spaces required
TOTAL		230 spaces	261
		Additional Parking Provided	38 tandem spaces 73 compact spaces (8'x16') 12 ADA accessible spaces 173 standard spaces
		TOTAL PROVIDED	557 parking spaces
		Grocery, Guest & Public Parking	210 parking spaces
		Reserved Resident Parking	347 parking spaces

Section 6-305 D. Approval criteria for P.A.D. (*in italics*):

1. *The development fulfills certain goals and objectives in the General Plan and the principles and guidelines of other area policy plans. Performance considerations are established to fulfill those objectives.* The downtown area has been seeking a grocery store for more than 20 years; the proposed project takes an existing infill lot and provides redevelopment of the property to meet the land use and economic development chapters of the General Plan by providing walkable/bike able access to healthy food, housing opportunity near employment, and a more sustainable and vibrant downtown community.
2. *Standards requested through the PAD Overlay district shall take into consideration the location and context for the site for which the project is proposed.* The project meets the Downtown Tempe height guidelines, is reflective of materials found in nearby developments, and serves as a transition between the downtown commercial core and the existing residential neighborhoods.
3. *The development appropriately mitigates transitional impacts on the immediate surroundings.* The proposed height is appropriate to the site in context with existing development in the immediate area.

USE PERMIT

The proposed use requires a use permit, to allow 38 tandem parking spaces.

Section 6-308 E Approval criteria for Use Permit (*in italics*):

1. *Any significant increase in vehicular or pedestrian traffic.* The site is proposing 38 tandem spaces within the reserved residential parking, to accommodate residents who may have a need for a second vehicle. The circulation for these spaces is entirely within the parking structure. Any traffic generated by the internal operations of the extraction of vehicles from these spaces would be self-contained within the residential portion of the garage. The number of spaces is relatively small and is not anticipated to impact traffic.
2. *Nuisance arising from the emission of odor, dust, gas, noise, vibration, smoke, heat or glare at a level exceeding that of ambient conditions.* The use of tandem spaces would not generate any more of the described conditions than vehicles parked side by side; the ambient conditions are contained within the garage and are adjacent to another existing parking structure, thereby not contributing to greater nuisance within the area.
3. *Contribution to the deterioration of the neighborhood or to the downgrading of property values, the proposed use is not in conflict with the goals objectives or policies for rehabilitation, redevelopment or conservation as set forth in the city's adopted plans or General Plan.* The proposed use of 38 tandem spaces out of a proposed 564 spaces contained within a parking structure is not anticipated to impact the surrounding property or neighborhood. These spaces are solely for the residents living on site, and provide the addition of spaces that exceed the requirements of the zoning code for downtown parking. Flexibility is allowed within the Planned Area Development model for redevelopment; and although parking could be added with another floor, increasing the height of the project; the applicant is proposing a parking solution that maximizes the footprint of the parking levels without increasing building height and keeping the building form within a transitional level compatible with the adjacent CenterPoint buildings.
4. *Compatibility with existing surrounding structures and uses.* The property to the north and to the north east of this site are parking garages, with minimal architectural enhancement, serving the commercial uses at CenterPoint. The proposed parking garage containing the tandem spaces is integrated into a vertical mixed-use development, minimizing the emphasis of the parking and reducing the lighting impacts from structured parking to adjacent areas. The proposed building incorporates materials found in and around the surrounding area, and the tandem parking spaces are wholly contained within the residential portions of the parking, not within the public parking area.
5. *Adequate control of disruptive behavior both inside and outside the premises which may create a nuisance to the surrounding area or general public.* The applicant has provided a parking management plan, and the site will be closely managed by on site leasing staff for the apartments and commercial staff of the grocery. The tandem spaces would be leased to residents within one unit, not split between units; therefore there should be no conflicts in the timing needs of residents in accessing their vehicles parked in tandem.

The manner of conduct and the building for the proposed use will not be detrimental to persons residing or working in the vicinity, to adjacent property, to the neighborhood, or to the public welfare in general, and that the use will be in full conformity to any conditions, requirement or standards prescribed therefore by this code.

DEVELOPMENT PLAN REVIEW

Site Plan

The site is oriented with two street frontages on the south and east sides with grocery storefront access on both elevations near the corner intersection. The site layout emphasizes alternative modes of transportation with a planned platform area for loading on the future street car as well as on-site bike parking for guests and grocery customers. The site design reduces potential conflicts with pedestrians, bicyclists and vehicles entering or exiting the site by locating the parking entrances on the

west end of the University frontage and the north end of the Ash frontage. Railroad quad gates are being provided at the railroad crossing for pedestrian/bike and vehicle control during Union Pacific railroad use of the tracks west of the site. These gates were required to be provided by the developer of this site, as part of the prior PAD and Development Agreement. The "back of house" uses of the ground floor grocery store are entirely enclosed, within the interior of the first floor parking garage; the circulation for refuse and deliveries is internal to the building. The setbacks are relatively small: the north side has a zero lot line configuration, pushing the building up to the northern limit, adjacent to an existing parking garage, without impact to the adjacent structure or use. The west side provides a five foot setback, three feet is required as an emergency egress path, but is designed with stabilized decomposed granite to reduce the amount of paving and the visual impact of a sidewalk not used except for emergency exiting, by blending the path with the landscape area to soften the edge of the building. The south side is set back four feet on the first floor, and extends out over the right of way at the top of the building. Landscape on this side is limited by existing utilities (water, storm drain and a 30" flood irrigation pipe) and required site visibility triangles for vehicles exiting the garage. The existing underground utilities would require the building to be set back approximately 12 additional feet from the property line to accommodate trees in ground at this location. The locations of the utilities were identified by blue stake and pot-hole exploration, determining the existing underground conditions. The east side is set back between two to ten feet on the first floor to allow additional landscape and enhance the street pedestrian area.

Building Elevations

The architecture has four unique but unified elevations, addressing different conditions on each side of the site. The street front facing University Drive has the parking entrance and utility equipment at the far west end of the site and uses a bronze anodized aluminum storefront glazing system and roll up glass garage doors along the first and second floors with a light tan masonry brick for massing and columns that relate to the masonry in CenterPoint and on Mill Avenue. The roll up doors serve a first floor patio and second floor mezzanine within the grocery store, allowing natural light and airflow into the grocery and opening the building as a gathering place for casual dining. A dark brown metal canopy is proposed at the pedestrian level entry canopy to the grocery. Level two through five uses a combination of solid and 51% open punched metal screening for the parking garage. The color of this metal panel is proposed to be grey and beige, blending with the other grey and beige building materials. Staff recommended changing this material to a mica-finished paint surface that provided a pearlized metallic finish, which would provide variation in the sunlight and more architectural interest. Due to the timing of receiving the elevation and material modifications, this change was not able to be incorporated. The applicant has indicated they wish to keep the grey and beige painted panel as proposed; staff is conditioning the screening to use metallic paint finish available within the product line. Levels six through ten of the 117' tall building are predominantly stucco with black metal railing, dark grey metal panels, and bronze vinyl windows with clear glazing. The stucco is integral colored for longer color durability and is proposed to be beige and variations of cool light, medium and dark toned greys that vary in projected rectangular wrapped forms and recessed portions with scored reveal patterning for additional shadow variation on the surfaces. An accent of crimson is used randomly under windows for vibrancy and color that breaks the architectural window pattern. Balconies provide private outdoor access from units and shade coverage to the units below. At the eighth floor level, a clubhouse with conference facilities, coffee bar, golf simulator and mini theater overlook the courtyard pool amenity below to the north, and University Drive to the south. The Clubhouse projects out from the upper level in a floating glass window wall with silver finished glazing that breaks up the residential elevation pattern.

The Ash Avenue side has the parking entrance at the north end, and screens the large electrical transformers behind a removable metal panel to allow utility access when necessary. The building materials on the east elevation are the same as the south, however the architectural application is slightly different. There are openings in the parking garage that create a checkerboard pattern between brick masonry and grey stucco panels. The primary upper level building color is the cool dark grey, accented with integral colored crimson-toned stucco color randomly placed on panels below the windows. Balconies project out from the façade to create shade for the units below, and are recessed from the street by the upper levels being set back from the garage level. The leasing office entry is wrapped with a metal band that extends along the base of the perforated metal garage screening, transitioning from this material to the storefront glazing below. The storefront glazing has what look like clerestory windows, above a shade canopy projecting from the mullion to scale the building to the pedestrian and provide shelter along the length of the building.

The north elevation is located on the property line for the first two floors, restricting any openings to meet building code requirements for fire protection of the adjacent property. At levels three through five, penetrations in the masonry wall wrap from the east side to the north side in a checkerboard pattern of openings that meet the allowed level of openings for the set

back from the property line and add shadows, rhythm and variation in the wall. The upper levels are predominantly the medium cool grey with beige used for the stucco wraps around elements to create projections. The parking screen wall is a combination of dark grey and crimson, creating woven bands of contrasting color in the openings between the precast garage wall. The precast material has two finishes, an integral colored smooth sand blasted finish with exposed mica, and an integral colored formed masonry block pattern with brick facing in a running bond pattern with grouting to match the masonry block on the south and east elevations.

On the west elevation, facing the railroad tracks, the lower level is again limited by proximity to the property line in the amount of openings. The applicant has provided as many openings as the building code will allow. The first five levels are precast concrete with a surface form of tan masonry brick with running bond patterning and grey grout lines. This product wraps from the north to the west side of the structure. The perforated metal product used to screen the garage on the east and south sides is wrapped at the southern end of this elevation, to provide visual continuity and more architectural interest in the corner visible from University Drive. Due to the large railroad property to the west, it is anticipated that this western elevation will remain visible due to lack of development opportunity. The upper levels have recessed windows, stucco wrap projections and balconies breaking up the pattern of the units. Dark grey and crimson stucco panels are used below the windows and medium cool grey stucco is the predominant building material on the upper floors. To shade the western side at the ground level, and provide some variation, a 2' planting area adjacent to the 3' stabilized path enables vertical green screens to allow plant growth on this elevation; a close up of this is shown on the color elevations. Plant material along this elevation will soften this side; provide variation in color and texture. The use of balconies above will add visual surveillance to this area.

The interior courtyard elevations are predominantly light grey stucco panels with reveals with a vertical emphasis. Dark grey and crimson colored stucco panels are used randomly above and below windows for variation in the patterning. This area will not be viewed from the public, but would be visible from the pool amenity area.

The project is not required to go through Art In Private Development, since the commercial component is less than 50,000 square feet (the zoning code does not have provisions for requirements of art in mixed use or multi-family projects, only on projects with a minimum of 50,000 square feet of commercial or industrial use).

The rooftop mounted HVAC equipment is proposed to be screened with the same 51% transparent punched metal product, in order to provide better air flow for mechanical system and also helps to reduce wind resistance for structural supports; however, this equipment would be visible through the screening proposed. Staff is conditioning that a solid panel or louvered product be used in lieu of the proposed perforated metal panel.

Landscape Plan

The 2% of site landscape area is designed to address the unique site conditions on the south, east and west perimeters. Along Ash Avenue the primary street tree is *Ulmus parvifolia*, Evergreen Elm, and the accent tree is *Lauris nobilis*, Bay Laurel. The landscape strips along the street edge include shade tolerant species Smooth Agave, Bush Morning Glory and Germander. Adjacent to the leasing office and grocery store, behind the walkway, are taller plants such as *Ruellia*, Purple *Muhlenbergia*, White *Plumbago* and Bush Morning Glory, providing a variety of colors, textures and plant materials with at least a 60% mature vegetative coverage.

Coordination with water utilities and Salt River Project (owner of the irrigation line) led to the proposed landscape design solution for University Drive. The south frontage along University Drive has several challenges impacting landscape, the constraints of the site include:

- Requirement for an 8' clear sidewalk (no light fixtures, benches, planters, etc.)
- Requirement for shade on the sidewalk
- Requirement to not have planters taller than 2' within the site visibility triangle for traffic exiting the site
- Requirement to not impact existing underground utility infrastructure.
- Requirement for the property owner to remove and replace any landscape material in the ROW if there are public works projects along the street front.

Although root barriers are permitted, direct planting on top of utilities is not permitted; the plants must be contained within large planters. The prior PAD established the setbacks, which are not being modified by this request. The proposed street

tree along University is Acacia anueura, Mulga, planted in five foot by five foot square movable planter boxes three feet tall to accommodate root growth. Bay Laurel accent trees are located in the ground adjacent to the building columns, as an accent to the patio facing the street front. Vegetation along the street front separates pedestrians from University Drive, defining the patio and sidewalk area with flowering plants and smooth-edged agaves.

The west side of the site has a different condition, requiring plants with high tolerance for direct sunlight and reflected heat along the masonry wall. Heavenly Bamboo, Bougainvillea, Catclaw Vine, Red Yucca and Purple Muhlenbergia provide a variety of colors and textures within a 2' planting strip next to a 3' stabilized decomposed granite egress path. Green screens are used for the Bougainvillea and Catclaw vines to help shade the side of the building and add vertical growth.

Section 6-306 D Approval criteria for Development Plan Review (*in italics*):

1. *Placement, form, and articulation of buildings and structures provide variety in the streetscape;* the building location maximizes the use of the site, entrances to the grocery provide easy access to pedestrians along both street frontages, serving both bus and street car transit users as well as cyclists with easy access to bicycle racks. The building articulation varies along the facades between different floor levels, set back from the property line at varied positions from two to ten feet, and overhangs projecting out to create shade and visual interest along the streetscape. The roofline is varied to provide greater articulation of the elevations from a distance.
2. *Building design and orientation, together with landscape, combine to mitigate heat gain/retention while providing shade for energy conservation and human comfort;* Use of recessed windows and projected balconies, wrapped building pop-outs and canopies provide shade on the street level as well as upper floors. Green screens are used on the west elevation and masonry penetrations along with decorative mesh used to screen the garage allows air flow through the third through fifth floors of the building. Trees are placed to maximize growth and shade on the east side, however the south side is limited in tree placement and species based on the underground utility constraints of the site.
3. *Materials are of a superior quality, providing detail appropriate with their location and function while complementing the surroundings;* materials provided include masonry brick veneer, exterior insulated stucco, vinyl windows for the residences and aluminum storefronts for the grocery store; all appropriate to the function and location of the development, in character with and complementing the surrounding materials.
4. *Buildings, structures, and landscape elements are appropriately scaled, relative to the site and surroundings;* the building is proposed to be ten stories; the parking structure to the north is six stories, being increased to eight stories. The proposed building is scaled appropriately within the downtown height guideline, which allows up to 300 feet in the downtown, and drops to 50 feet west of the railroad tracks. The project is an appropriate transition from the downtown to the developments west of the railroad. The landscape elements are scaled for a pedestrian experience relative to the building elevations.
5. *Large building masses are sufficiently articulated so as to relieve monotony and create a sense of movement, resulting in a well-defined base and top, featuring an enhanced pedestrian experience at and near street level;* the building has distinctive elevations on all four sides, using similar materials and forms for continuity, and the use of windows, columns and façade changes to create a sense of movement. The building massing is articulated through changes in vertical and horizontal plane as well as in color and material.
6. *Building facades provide architectural detail and interest overall with visibility at street level (in particular, special treatment of windows, entries and walkways with particular attention to proportionality, scale, materials, rhythm, etc.) while responding to varying climatic and contextual conditions;* the use of landscape and building material provides visual cues to the façade for interaction with the pedestrian, with enhancement to the entryways through changes in landscape materials and use of shade and lighting.
7. *Plans take into account pleasant and convenient access to multi-modal transportation options and support the potential for transit patronage;* the plan accommodates the future street car lane with transit stop and accessible bike and vehicle parking.

8. *Vehicular circulation is designed to minimize conflicts with pedestrian access and circulation, and with surrounding residential uses; vehicles are relegated to the outside perimeters of the site, creating a uniform walkable frontage along both University and Ash Avenue, special consideration was given to site visibility to and from the garage due to the high pedestrian and bicycle movements anticipated along both streets.*
9. *Plans appropriately integrate Crime Prevention Through Environmental Design principles such as territoriality, natural surveillance, access control, activity support, and maintenance; the design meets all public safety requirements, activates the street frontages and provides natural surveillance from the street front patio, upper floor mezzanine with roll up doors, and the balconies and windows on the floors above.*
10. *Landscape accents and provides delineation from parking, buildings, driveways and pathways; landscape is appropriate to the conditions of the site, defines the pedestrian paths and building entryways and provides color, texture and variety within a limited landscape area.*
11. *Signs have design, scale, proportion, location and color compatible with the design, colors, orientation and materials of the building or site on which they are located; signs will be handled by separate application and are not a part of this request, they were provided for illustrative purposes only.*
12. *Lighting is compatible with the proposed building(s) and adjoining buildings and uses, and does not create negative effects.* Lighting provided was in excess of the code requirements and staff has provided recommendations for lighting level reductions both for prevention of glare and reduction of energy consumption.

Conclusion

Based on the information provided and the above analysis, staff recommends approval of the requested Planned Area Development, Use Permit and Development Plan Review. This request meets the required criteria and will conform to the conditions.

REASONS FOR APPROVAL:

1. The project meets the General Plan Projected Land Use and Projected Residential Density for this site.
2. The project will meet the development standards required under the Zoning and Development Code.
3. The PAD overlay process was specifically created to allow for greater flexibility.
4. The proposed project meets the approval criteria for a Planned Area Development, Use Permit and Development Plan Review.

ZONING AMENDMENT AND PLANNED AREA DEVELOPMENT CONDITIONS OF APPROVAL:

EACH NUMBERED ITEM IS A CONDITION OF APPROVAL. THE DECISION-MAKING BODY MAY MODIFY, DELETE OR ADD TO THESE CONDITIONS.

General

1. A building permit application shall be made within two years of the date of City Council approval or the zoning of the property may revert to that in place at the time of application. Any reversion is subject to a public hearing process as a zoning map amendment.
2. The property owner(s) shall sign a waiver of rights and remedies form. By signing the form, the Owner(s) voluntarily waive(s) any right to claim compensation for diminution of Property value under A.R.S. §12-1134 that may now or in the future exist, as a result of the City's approval of this Application, including any conditions, stipulations and/or modifications imposed as a condition of approval. The signed form shall be submitted to the Community Development Department no later than 30 days from the date of City Council approval, or the Planned Area Development approval shall be null and void.
3. The Planned Area Development Overlay for THE FOUNDRY shall be put into proper engineered format with appropriate signature blanks and kept on file with the City of Tempe's Community Development Department within sixty (60) days of

the date of City Council approval.

4. The developer must receive approval of the final Traffic Impact Study from the Traffic Engineering prior to issuance of a building permit.
5. The parking management plan must be updated to the most current parking numbers identified in the plans submitted.

USE PERMIT CONDITIONS OF APPROVAL:

1. This Use Permit for 38 tandem parking spaces will be null and void unless the required inspections for the certificate of occupancy are completed.
2. The Use Permit is valid for the plans as submitted within this application. Any additions or modifications may be submitted for review during building plan check process.
3. Any intensification or expansion of the 38 tandem spaces shall require a new Use Permit.
4. No compact spaces may be used in the allocation of these tandem spaces.

DEVELOPMENT PLAN REVIEW CONDITIONS OF APPROVAL:

General

1. Except as modified by conditions, development shall be in substantial conformance with the site plan and building elevations dated May 6, 2016 and landscape plan dated May 6, 2016. Minor modifications may be review through the plan check process of construction documents; major modifications will require submittal of a Development Plan Review.

Site Plan

2. Interior building walls, ceilings, and floors for the residential units shall provide a minimum sound transmission class of (55) or more. Exterior building walls for the residential units shall provide a minimum sound transmission class of (39) or more. Exterior windows for the residential units shall provide a minimum sound transmission class of (28) or more using insulated double paned windows with ¼" pane thickness or more.
3. Provide service locations for both refuse and recycling collection and pick-up on the property as presented. Coordinate the storage areas for refuse and recycling containers with the overall site and landscape layout.
4. Provide service yard and mechanical yard or parapet walls that are at least the height of the equipment being enclosed. Verify height of equipment and mounting base to ensure that wall height is adequate to fully screen the equipment.
5. Provide upgraded paving at each driveway consisting of integral colored unit paving. Extend this paving in the driveway from the right-of-way line to 20'-0" on site and from curb to curb at the drive edges. From sidewalk to right-of-way line, extend concrete paving to match sidewalk.
6. Utility equipment boxes for this development shall be screened from street view by architectural integration with the building elevations and finished in a neutral color (subject to utility provider approval) that compliments the coloring of the buildings.
7. Place exterior, freestanding reduced pressure and double check backflow assemblies in pre-manufactured, pre-finished, lockable cages (one assembly per cage). If backflow prevention or similar device is for a 3" or greater water line, delete cage and provide a masonry or concrete screen wall following the requirements of Standard Detail T-214.

Floor Plans

8. Exit Security:
 - a. Provide visual surveillance by means of fire-rated glazing assemblies from stair towers into adjacent circulation spaces.

- b. In instances where an elevator or stair exit is within 21'-0" of an alcove, corner or other potential hiding place, position a refracting mirror to allow someone in the exit doorway to observe in the mirror the area around the corner or within the alcove that is adjacent to the doorway.
9. Public Restroom Security:
- a. Lights in restrooms:
 - 1) Provide 50% night lights
 - 2) Activate by automatic sensors, key or remote control mechanism
 - b. Single user restroom door hardware:
 - 3) Provide a key bypass on the exterior side
10. Garage Security:
- a. Minimize interior partitions or convert these to semi-opaque screens to inhibit hiding behind these features.
 - b. Provide exit stairs that are open to the exterior as indicated.
 - c. Maximize openness at the elevator entrances and stair landings to facilitate visual surveillance from these pedestrian circulation areas to the adjacent parking level.
11. Parking Garage:
- a. Minimum required parking dimensions shall be clear of any obstructions.
 - b. At the ends of dead-end drive aisles, provide a designated turn-around space, minimum 8'-6" clear in width (locate on left side if available), including 3'-0" vehicular maneuvering area for exiting. Turn-around area shall be clearly demarcated.
 - c. Provide a minimum 2'-0" of additional width for parking spaces when adjacent to a continuous wall.
 - d. Paint interior wall and overhead surfaces in garage floor levels with a highly reflective white color, minimum LRV of 75 percent.
 - e. Compact spaces as indicated on plans are 8' x 16' in dimension, and are not accounted for as required parking.
 - f. There shall be no more than 85 compact spaces.
 - g. Compact spaces shall be signed for compact vehicles and motorcycles/scooters only.
 - h. Per Sheet A411-A loading diagram, the refuse collection and delivery area shall be a minimum of 25' vertical clearance and the drive aisle shall be a minimum of 15' vertical clearance without any obstructions.

Building Elevations

12. The materials and colors are approved as presented (May 9, 2016):

Material Board

B - Primary Building	- stucco 3- coat system integral color to match Sherwin Williams SW7011 Gray Screen (cool grey)
C - Secondary Building	- stucco 3- coat system integral color to match Sherwin Williams SW7075 Web Grey (medium grey)
D - Tertiary Building	- Masonry Summit – Victorian – (tan)
K - Quaternary Building	- stucco 3- coat system integral color to match Sherwin Williams SW7036 Accessible Beige (light beige)
E - Building Accent	- Metal Panel Morin Blue Gray (dark cool grey)
J - Building Accent	- stucco 3- coat system integral color to match Sherwin Williams SW6321 Red Bay (crimson red)
A - Garage Screening	- Fluoropon PVDF Kynar 500 Surrey Beige and Dove Grey 51% transparency with 3/4" diameter punched alternate circular perforation. South east corner elevation is solid panel product to be replaced with Mica Fluorspan Classic II PVDF colors, Silversmith 399C1887 and Gingerale 399B045 (or similar) for the 2 tone pattern on the perforated and solid portions of the garage screening and grocery entrance.
M - Mechanical Screening	- must meet screening requirements; Fluoropon PVDF Kynar 500 Dove Grey solid panel or equivalent for screening; product may be louvered for wind load and ventilation requirements.

G - Storefront Windows - Anodized aluminum frame - EFCO Series 403 – Dark Bronze
 F - Residential Windows – Vinyl frame – Fibrex Dark Bronze – clear dual panel low-e
 H - Railing – powder coated black finish metal railing (black)
 L - Metal canopy – Morin Spartan Bronze
 N – Accent garage (North and West elevation) - Precast panel finish color integral color to match Sherwin Williams SW7036 Accessible Beige (light beige) with light sandblast finish
 O –Primary garage (North and West elevation) – Precast panel finish to match masonry Summit Victorian with running bond patterned grouted masonry finish (tan).
 I - Letter Not Used
 Window Wall System - Viracon – Silver VFE 1-38
 Roof – Flat with parapet
 Provide primary building colors and materials with a light reflectance value of 75 percent or less. Additions or modifications may be submitted for review during building plan check process.

13. Garage levels shall provide a minimum of 2'9" high vehicle screening on exterior of building, behind proposed punched metal screen.
14. Provide secure roof access from the interior of the building. Do not expose roof access to public view.
15. Conceal roof drainage system within the interior of the building.
16. Incorporate lighting, address signs, and incidental equipment attachments (alarm klaxons, security cameras, etc.) where exposed into the design of the building elevations. Exposed conduit, piping, or related materials is not permitted.
17. Locate the electrical service entrance section (S.E.S.) inside the building or inside a secure yard that is concealed from public view.
18. Upper/lower divided glazing panels in exterior windows at grade level, where lower glass panes are part of a divided pane glass curtain-wall system, shall be permitted only if laminated glazing at these locations is provided.

Lighting

19. Illuminate building entrances and underside of open stair landings from dusk to dawn to assist with visual surveillance at these locations.

Landscape

20. The plant palette is approved as proposed and specified on the landscape plan. Any additions or modifications may be submitted for review during building plan check process.
21. Arterial street trees shall be a minimum of 36" box specimens and a minimum of 1 ½" caliper trunk.
22. Irrigation notes:
 - a. Provide dedicated landscape water meter.
 - b. Provide pipe distribution system of buried rigid (polyvinylchloride), not flexible (polyethylene). Use of schedule 40 PVC mainline and class 315 PVC ½" feeder line is acceptable. Class 200 PVC feeder line may be used for sizes greater than ½". Provide details of water distribution system.
 - c. Locate valve controller in a vandal resistant housing.
 - d. Hardwire power source to controller (a receptacle connection is not allowed).
 - e. Controller valve wire conduit may be exposed if the controller remains in the mechanical yard.
23. Include requirement to de-compact soil in planting areas on site and in public right of way and remove construction debris from planting areas prior to landscape installation.
24. Top dress planting areas with a rock or decomposed granite application. Provide rock or decomposed granite of 2"

uniform thickness. Provide pre-emergence weed control application and do not underlay rock or decomposed granite application with plastic.

25. Trees shall be planted a minimum of 20'-0" from any existing or proposed public water or sewer lines. The tree planting separation requirements may be reduced from the waterline upon the installation of a linear root barrier, a minimum of 6'-0" parallel from the waterline, or around the tree. The root barrier shall be a continuous material, a minimum of 0.08" thick, installed 0'-2" above finish grade to a depth of 8'-0" below grade. Final approval subject to determination by the Public Works, Water Utilities Division.

Addressing

26. Provide address numerals on the north, west and south building elevation facing the street to which the property is identified.
 - a. Conform to the following for building address signs:
 - 1) Provide street number only, not the street name
 - 2) Compose of 12" high, individual mount, metal reverse pan channel characters.
 - 3) Self-illuminated or dedicated light source.
 - 4) Coordinate address signs with trees, vines, or other landscaping, to avoid any potential visual obstruction.
 - 5) Do not affix number or letter to elevation that might be mistaken for the address.
 - b. Utility meters shall utilize a minimum 1" number height in accordance with the applicable electrical code and utility company standards.
 - c. Provide one address sign on the roof of the building. Orient sign to be read from the south.
 - 1) Include street address number in 6'-0" high characters on one line and street name in 3'-0" high characters on a second line immediately below the first.
 - 2) Provide high contrast sign, either black characters on a light surface or white characters on a black field that is painted on a horizontal plane on the roof. Coordinate roof sign with roof membrane so membrane is not compromised.
 - 3) Do not illuminate roof address.

CODE/ORDINANCE

REQUIREMENTS:

THE BULLETED ITEMS REFER TO EXISTING CODE OR ORDINANCES THAT PLANNING STAFF OBSERVES ARE PERTINENT TO THIS CASE. THE BULLET ITEMS ARE INCLUDED TO ALERT THE DESIGN TEAM AND ASSIST IN OBTAINING A BUILDING PERMIT AND ARE NOT AN EXHAUSTIVE LIST.

- Development plan approval shall be void if the development is not commenced or if an application for a building permit has not been submitted, whichever is applicable, within twelve (12) months after the approval is granted or within the time stipulated by the decision-making body. The period of approval is extended upon the time review limitations set forth for building permit applications, pursuant to Tempe Building Safety Administrative Code, Section 8-104.15. An expiration of the building permit application will result in expiration of the development plan.
- Specific requirements of the **Zoning and Development Code (ZDC)** are not listed as a condition of approval, but will apply to any application. To avoid unnecessary review time and reduce the potential for multiple plan check submittals, become familiar with the ZDC. Access the ZDC through www.tempe.gov/zoning or purchase from Community Development.
- **SITE PLAN REVIEW:** Verify all comments by the Public Works Department, Community Development Department, and Fire Department given on the Preliminary Site Plan Review. If questions arise related to specific comments, they should be directed to the appropriate department, and any necessary modifications coordinated with all concerned parties, prior to application for building permit. Construction Documents submitted to the Building Safety Division will be reviewed by planning staff to ensure consistency with this Design Review approval prior to issuance of building permits.
- **STANDARD DETAILS:**
 - Access to Tempe Supplement to the M.A.G. Uniform Standard Details and Specifications for Public Works Construction, at this link: <http://www.tempe.gov/city-hall/public-works/engineering/standards-details> or purchase book from the Public Works Engineering Division.

- Access to refuse enclosure details DS116 and DS118 and all other Development Services forms at this link: <http://www.tempe.gov/city-hall/community-development/building-safety/applications-forms>. The enclosure details are under Civil Engineering & Right of Way.
- BASIS OF BUILDING HEIGHT: Measure height of buildings from top of curb at a point adjacent to the center of the front property line.
- COMMUNICATIONS:
 - Provide emergency radio amplification for the combined building and garage area in excess of 50,000 sf. Amplification will allow Police and Fire personnel to communicate in the buildings during a catastrophe. Refer to this link : <http://www.tempe.gov/home/showdocument?id=30871>. Contact the Information Technology Division to discuss size and materials of the buildings and to verify radio amplification requirements.
 - For building height in excess of 50'-0", design top of building and parapet to allow cellular communications providers to incorporate antenna within the building architecture so future installations may be concealed with little or no building elevation modification.
- PUBLIC ART: If the commercial portion of the project meets or exceeds 50,000 square feet, provide public art for this development in conformance with the Art in Private Development Ordinance and ZDC Sec. 4-407 and ZDC Appendix D. Contact the Community Services Cultural Services Division regarding implementation of this requirement prior to receiving building permits.
- WATER CONSERVATION: Under an agreement between the City of Tempe and the State of Arizona, Water Conservation Reports are required for landscape and domestic water use for the non-residential components of this project. Have the landscape architect and mechanical engineer prepare reports and submit them with the construction drawings during the building plan check process. Report example is contained in Office Procedure Directive # 59. Refer to this link: www.tempe.gov/modules/showdocument.aspx?documentid=5327. Contact Public Works Department, Water Conservation Division with questions regarding the purpose or content of the water conservation reports.
- HISTORIC PRESERVATION: State and federal laws apply to the discovery of features or artifacts during site excavation (typically, the discovery of human or associated funerary remains). Contact the Historic Preservation Officer with general questions. Where a discovery is made, contact the Arizona State Historical Museum for removal and repatriation of the items.
- POLICE DEPARTMENT SECURITY REQUIREMENTS:
 - Refer to Tempe City Code Section 26-70 Security Plans.
 - Design building entrance(s) to maximize visual surveillance of vicinity. Limit height of walls or landscape materials, and design columns or corners to discourage ambush.
 - Maintain distances of 20'-0" or greater between a pedestrian path of travel and any hidden area to allow for increased reaction time and safety.
 - Follow the design guidelines listed under appendix A of the Zoning and Development Code. In particular, reference the CPTED principal listed under A-II Building Design Guidelines (C) as it relates to the location of pedestrian environments and places of concealment.
 - Provide method of override access for Police Department (punch pad or similar) to controlled access areas including pool, clubhouse or other gated common areas.
 - The Owner is required to prepare a security plan for the project with the Police Department. The architect should be involved to verify any modification that would require design revisions. To avoid revisions to permitted construction documents, initial meetings with the Police Department regarding the security plan are recommended before building permits are issued. At a minimum, the Owner shall contact the Police Department to begin security plan process approximately eight weeks prior to receipt of certificate of occupancy.
 - In conjunction with the security plan, Crime Free Multi-Housing status for this property may be required.
 - Provide a security vision panel at service and exit doors (except to rarely accessed equipment rooms) with a 3" wide high strength plastic or laminated glass window, located between 43" and 66" from the bottom edge of the door.

- TRAFFIC ENGINEERING:
 - Provide 8'-0" wide public sidewalk along arterial roadways, or as required by Traffic Engineering Design Criteria and Standard Details.
 - Incorporate brick sidewalks for all off-site pedestrian paving. Follow City of Tempe Public Works Department Detail T-353, when designing all sidewalk areas in the Right-of-Way. Alternative paver materials may be considered subject to review, and approval, by the Engineering and Planning Departments. Any alternative patterns should be used in small amounts to create accent areas at entrances, or to demarcate architectural features of the building. Do not propose a wholesale change of material. These materials shall be compatible with the Americans with Disabilities Act, ADA, and the Building Code.
 - Construct driveways in public right of way in conformance with Standard Detail T-320.
 - Correctly indicate clear vision triangles at both driveways on the site and landscape plans. Identify speed limits for adjacent streets at the site frontages. Begin sight triangle in driveways at point 15'-0" in back of face of curb. Consult Intersection Sight Distance memo, available from Traffic Engineering if needed www.tempe.gov/index.aspx?page=801 . Do not locate site furnishings, screen walls or other visual obstructions over 2'-0" tall (except canopy trees are allowed) within each clear vision triangle.
- FIRE:
 - Clearly define the fire lanes. Ensure that there is at least a 20'-0" horizontal width, and a 14'-0" vertical clearance from the fire lane surface to the underside of tree canopies or overhead structures. Layout and details of fire lanes are subject to Fire Department approval.
 - Provide a fire command room(s) on the ground floor of the building(s). Verify size and location with Fire Department.
- CIVIL ENGINEERING:
 - An Encroachment Permit or License Agreement must be obtained from the City for any projections into the right of way or crossing of a public utility easement, prior to submittal of construction documents for building permit.
 - Maintain a minimum clear distance of twenty-four (24) feet between the sidewalk level and any overhead structure.
 - Underground utilities except high-voltage transmission line unless project inserts a structure under the transmission line.
 - Coordinate site layout with Utility provider(s) to provide adequate access easement(s).
 - Clearly indicate property lines, the dimensional relation of the buildings to the property lines and the separation of the buildings from each other.
 - Verify location of any easements, or property restrictions, to ensure no conflict exists with the site layout or foundation design.
 - The site is within an Alternative Retention Criteria Area. Verify specific design considerations with the Engineering Department.
- SOLID WASTE SERVICES:
 - Enclosure indicated on site plan is exclusively for refuse. Construct walls, pad and bollards in conformance with standard detail DS-116.
 - Contact Public Works Sanitation Division to verify that vehicle maneuvering and access to the enclosure is adequate. Refuse staging, collection and circulation must be on site; no backing onto or off of streets, alleys or paths of circulation.
 - Develop strategy for recycling collection and pick-up from site with Sanitation. Coordinate storage area for recycling containers with overall site layout.
 - Gates for refuse enclosure(s) are not required, unless visible from the street. If gates are provided, the property manager must arrange for gates to be open from 6:00am to 4:30pm on collection days.
- PARKING SPACES:
 - Verify conformance of accessible vehicle parking to the Americans with Disabilities Act and the Code of Federal Regulations Implementing the Act. Refer to Building Safety ADA Accessible Parking Spaces Marking/Signage on Private Development details.

- At parking areas, provide demarcated accessible aisle for disabled parking.
- Distribute bike parking areas nearest to main entrance(s). Provide parking loop/rack per standard detail T-578. Provide 2'-0" by 6'-0" individual bicycle parking spaces. One loop may be used to separate two bike parking spaces. Provide clearance between bike spaces and adjacent walkway to allow bike maneuvering in and out of space without interfering with pedestrians, landscape materials or vehicles nearby.
- LIGHTING:
 - Design site security light in accordance with requirements of ZDC Part 4 Chapter 8 (Lighting) and ZDC Appendix E (Photometric Plan).
 - Indicate the location of all exterior light fixtures on the site, landscape and photometric plans. Avoid conflicts between lights and trees or other site features in order to maintain illumination levels for exterior lighting.
- LANDSCAPE:
 - Prepare an existing plant inventory for the site and adjacent street frontages. The inventory may be prepared by the Landscape Architect or a plant salvage specialist. Note original locations and species of native and "protected" trees and other plants on site. Move, preserve in place, or demolish native or "protected" trees and plants per State of Arizona Agricultural Department standards. File Notice of Intent to Clear Land with the Agricultural Department. Notice of Intent to Clear Land form is available at www.azda.gov/ESD/nativeplants.htm . Follow the link to "applications to move a native plant" to "notice of intent to clear land".
- SIGNS: Separate plan review process is required for signs in accordance with requirements of ZDC Part 4 Chapter 9 (Signs). Refer to www.tempe.gov/signs.

HISTORY & FACTS:

- November 26, 1894 the site was part of the original Tempe. In 1914 the president of the Salt River Valley Cotton Growers Association joined with several Tempe businessmen to establish Tempe Cotton Exchange and Seed house, and Tempe's first cotton gin. The site served for industry research in the use of Pima Cotton, long-staple cotton competitive with Egyptian cotton.
- May 21, 1974 Board of Adjustment approved the request for a use permit for a restaurant and a variance to reduce the street side yard setback from 12 feet to 0 feet within the I-2, General Industrial District.
- April 14, 1975 Board of Adjustment approved the request by Dr. Munchies for a use permit for a restaurant and a variance to reduce the street side yard setback from 12 feet to 0 feet within the I-2, General Industrial District located at 234 West University Drive.
- August 13, 1976 Board of Adjustment approved the request for Dr. Munchies Restaurant to amend the variance to reduce the street side yard setback from 12 feet to 0 feet for an existing building to a variance to reduce the front yard setback from 25 feet to 0 feet for an existing building and proposed additions.
- October 23, 1980 Board of Adjustment approved the request by Coyote Junction for a use permit for a restaurant use in an industrial district and a use permit to allow entertainment as an accessory use to a restaurant.
- May 24, 1984 Board of Adjustment approved the request by Gentle Strength Coop for a use permit for a use permit to operate a commercial use in an industrial district; variance to reduce the required front yard setback from 25 feet to 0 feet; and a variance to reduce the required street side yard setback from 15 feet to 12 feet.
- January 23, 1985 Board of Adjustment approved the request by Gentle Strength to waive the required screening wall for outdoor storage (condition: valid for only 90 days) at 234 West University Dr.
- February 27, 1991 Board of Adjustment approved the request by Gentle Strength for a use permit to expand the existing restaurant building at 234 West University Dr.
- 1991 The octagonal cotton seed house, the last of the structures from the historic cotton gin use was torn down in 1991.

- April 18, 1995 Hearing Officer approved the request by Gentle Strength for a use permit to expand the facility by enclosing two areas at 234 West University Dr.
- March 15, 2005 Redevelopment Review Commission heard a request for a Zoning Map Amendment and Planned Area Development for Cosmo #RRC05004, for a sixteen story (207 feet tall) building with approximately 488,000 s.f. of building located at 234 West University Drive, including the following:
- #ZON-2005.05 Ordinance No. 2005.08 for a Zoning Map Amendment from GID, General Industrial District (formerly I-2, General Industrial) to CC, City Center District, pursuant to Section 2-106 of the Zoning and Development Code.
- #SPD-2005.32 for a Preliminary Planned Area Development Overlay District consisting of 170,000 s.f. of residential condominiums, 67,000 s.f. of grocery store/commercial and 450+ parking spaces within 488,000 s.f. of building on 1.77 acres, pursuant to Section 2-106 of the Zoning and Development Code.
- May 19, 2005 City Council held a first public hearing for the above described request.
- June 2, 2005 City Council approved the request by Cosmo for a Zoning Map Amendment from GID, General Industrial District to CC, City Center District and a Preliminary Planned Area Development Overlay.
- March 23, 2006 City Council approved a Subdivision Plat request by COSMOPOLITAN #SBD-2006.26 for an Amended Subdivision Plat, consisting of two (2) lots on 3.84 net acres, located at 234 W. University Drive.
- June 6, 2006 City Council approved a Planned Area Development Overlay and Development Plan Review including building elevations, site and landscape plans, for an eighteen (18) story building with approximately 733,000 s.f. of building for commercial and residential use, located at 234 West University Drive, in the CC, City Center District, including the following:
- #SPD-2006.53 Ordinance No. 2006.49 for a Planned Area Development Overlay for 733,000 s.f. mixed-use building.
 - Modify Condition of Approval #11 (SPD-2005.32), to allow a maximum building height from (207) feet to (235) feet.
- May 17, 2006 Development Services administratively approved the 12th Amended Planned Development Overlay for CenterPoint and an Amended Final PAD for Phase Two – Parking Garage No. 2, in order to exclude a portion of the PAD for the purpose of developing the KML Cosmopolitan project on 234 West University Drive.
- August 7, 2007 Hearing Officer approved a request by MOSAIC (PL070092) for a Use Permit to allow tandem parking for a residential use located at 770 S Ash Avenue. The name of the project and the street address changed, but the site was the former Cosmo project site.
- May 24, 2016 Development Review Commission is scheduled for a public hearing for an Amended Planned Area Development, Use Permit for Tandem Parking and a Development Plan Review.
- June 9, 2016 City Council is scheduled for an introduction and first public hearing.
- June 23, 2016 City Council is scheduled for a second public hearing.

ZONING AND DEVELOPMENT CODE REFERENCE:

Section 6-305, Planned Area Development (PAD) Overlay districts
 Section 6-306, Development Plan Review
 Section 6-308, Use Permit



DEVELOPMENT PROJECT FILE

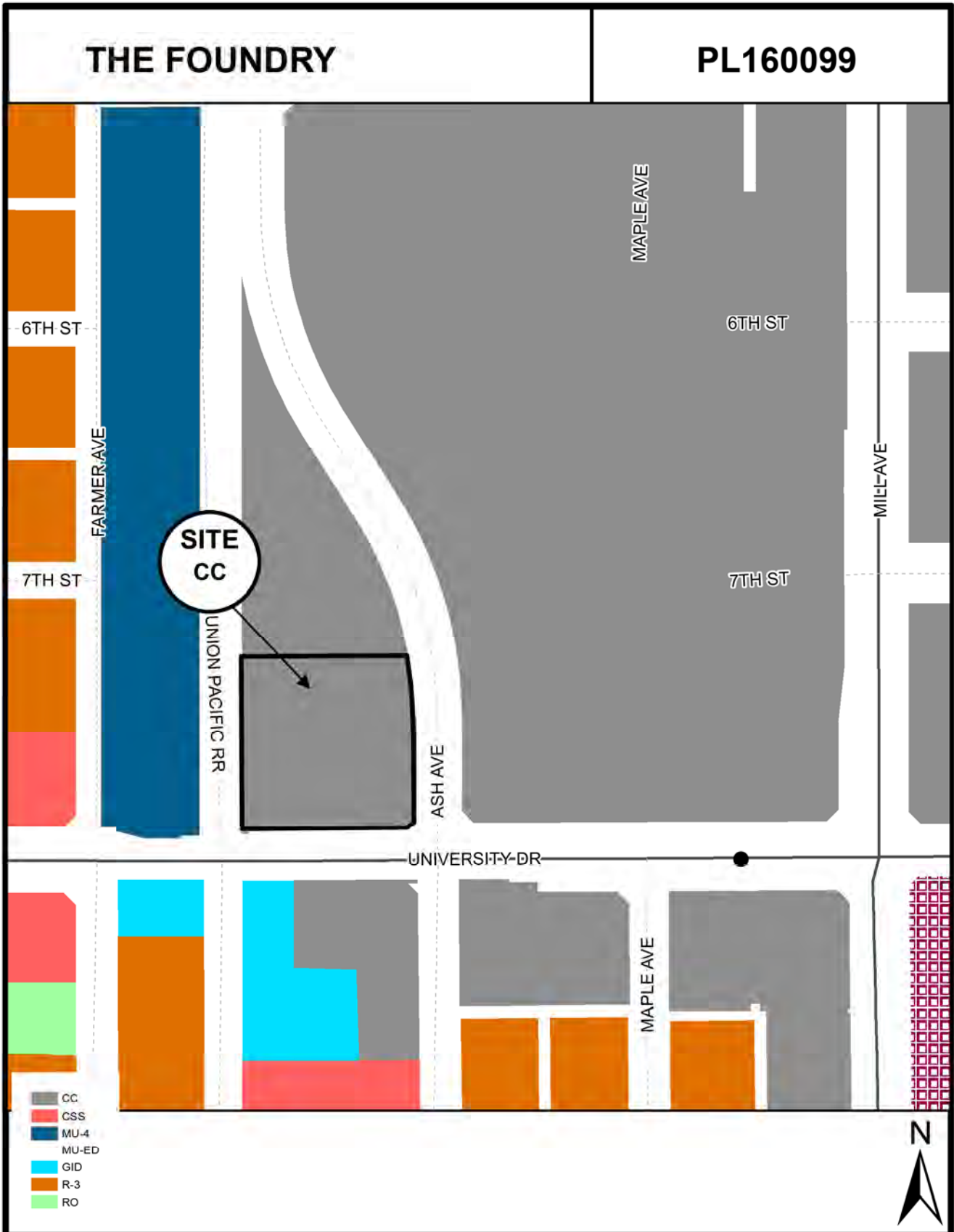
for

THE FOUNDRY

ATTACHMENTS:

1. Location Map
2. Aerial
- 3-37. Letter of Explanation
38. Planned Area Development Cover Sheet A101
39. PAD Site Plan Sheet A102
40. Enlarged Site Plan Sheet A103
- 41-42. Enlarged Development Standards Chart
43. Massing Diagram Sheet A104
44. First Floor Plan Sheet A411
45. Traffic Diagram Sheet A110
46. Enlarged Loading Area Diagram Sheet A411a
47. Mezzanine Level Floorplan of Grocery Store Sheet A412
48. Level 2 Floor Plan of Parking Garage Sheet A413
49. Level 3 Floor Plan of Parking Garage Sheet A414
50. Level 4 Floor Plan of Parking Garage Sheet A415
51. Level 5 Floor Plan Residential Units and Amenities (Lazy River & Pool, Fitness Room & Conference Room) Sheet A416
52. Levels 6-7 Floor Plans of Residences Sheet A417
53. Level 8 Floor Plan of Residences and Amenities Coffee Bar, Conference Room, Mini Theater, Golf Simulator and Clubhouse Lounge/Game Room Sheet A418

54. Level 9 Floor Plan of Residences Sheet A419
55. Roof Plan Sheet A420
- 56-59. Unit Floor Plans Sheets A301 & A302 (3 pages labeled A302)
60. Building Sections Sheet A501
61. Landscape Plan Ground Level Sheet L1
62. Landscape Amenity Deck Sheet L2
63. Building Perspective of South East Corner with Material Callouts Sheet A510
64. Blackline South and East Building Elevations Sheet A515
65. Blackline North and West Building Elevations Sheet A516
- 66-67. Blackline Courtyard Elevations Sheets A517-A518
68. Color South and East Elevations Sheet A511
69. Color West and North Elevations Sheet A512
- 70-71. Color Courtyard Elevations Sheets A513-A514
72. Southeast Corner Perspective Rendering Sheet A105
73. Close up of University Drive & Ash Avenue Pedestrian Level Perspective Sheet A106
74. Diagram of Patio and Mezzanine Area of Grocery in Section and Perspective Sheet A107
75. Close up Rendering of Grocery Patio and Mezzanine at Street Level Sheet A107a
76. Ashe Avenue Perspective Sheet A108
77. Garage Opening Calculations for Building Code Compliance
- 78-80. Material Sample Boards
- 81-84. Public Involvement Summary Report
- 85-89. Context Photos



Location Map

THE FOUNDRY

PL160099



Aerial Map

**THE FOUNDRY
ENTITLEMENT APPLICATION
PAD, DPR, & USE PERMIT**

PREPARED FOR:
FORUM REAL ESTATE GROUP, LLC
DAVID CLOCK
MANAGER - DEVELOPMENT

**NWC UNIVERSITY DR. & ASH AVE.
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HUMPHREYS & PARTNERS URBAN ARCHITECTURE, L.P.



**THE FOUNDRY
PROJECT NARRATIVE**

I. INTRODUCTION

Forum Real Estate Group (“Forum”) and their development partner Alberta Development Partners (“Alberta”) respectfully submits the following entitlement request for The Foundry, a mixed-use mid-rise infill project poised to redevelop the vacant northwest corner of University Drive and Ash Avenue (the “Site”). The Foundry includes a ±40,000 square foot Whole Foods Market and Class A+ residential units with structured parking in a modern 9-story building. Alberta will partner with Forum who will develop, operate, and own the property.



A ground-level, 2-story volume Whole Foods Market anchors the development to the Site, with three levels of structured parking and five levels of apartment homes rising above the base. A variety of design elements work together to activate the pedestrian realm. Large, roll-up doors along University Drive open the ground level to outdoor dining areas for Whole Foods patrons. This outdoor dining experience is continued above at the mezzanine level. The store-front window façade enhances visual permeability and the roll-up doors create openings that blend the inside with outside. Accommodations

for the urban streetcar, including a defined stop shaded by the building canopies above, are provided on Ash Avenue.

Three levels of structured parking sit above the ground level retail. These three levels will be screened using a metal mesh material that will be artistic in nature and incorporate a Downtown Tempe vibe. Five levels of luxury apartment homes arranged around a resort-quality pool and resident amenity deck rise above the building base. The residential element of the building features projections and balconies that combine with a variety of building materials to create articulation and provide rhythm and movement in the elevation.

The Foundry’s target demographic is diverse, educated, and savvy. Intended for an urban dweller market that rents by choice, The Foundry’s apartment homes are geared towards a mix of working professionals of all ages. The primary target market of mature Millennials includes young professionals, recent graduates, young singles and couples, graduate and international students, and relocated professionals. A secondary resident demographic includes working professionals and empty nesters.

Whole Foods Market is the world’s leader in natural and organic foods, with 440 stores in North America and the United Kingdom with over 85,000 team members. They seek out the finest natural and organic



foods available, maintain the strictest quality standards in the industry, and have an unshakeable commitment to sustainable

agriculture. Whole Foods Market has grown from a single, small natural and organic foods store to a chain recognized nationally and internationally for high standards and amazing quality. Some of their stores are largely unchanged from



First Whole Foods store.

the early days of their company and are neighborhood fixtures. Their newer stores are larger and offer a greater selection of products, but they continue to respect the neighborhood to which they are moving and strive to become an integral part of the community. They like to think that it isn't just the food that makes shopping at Whole Foods Market a great experience, but the store itself is a part of the experience. The architecture, the location, and layout of the products available to their shoppers - every single store is unique.

A. REQUEST OVERVIEW – ENTITLEMENTS

To achieve the development vision, Forum seeks a Planned Area Development overlay (PAD) amendment to modify the existing approved PAD, a Development Plan Review (DPR) for the new buildings, landscaping, and site plan, and a Use Permit for tandem parking.

B. PROJECT LOCATION, GENERAL PLAN, AND ZONING

The ±1.86 acre Site is a vacant lot located at 204 West University Drive, the northwest corner of University Drive and Ash Avenue. The Site comprises Lot 1 of the 2006/2007 Cosmopolitan plat, and is bound by University Drive on the south, the Southern Pacific Railroad on the west, a privately owned parking structure to the north, and Ash Avenue to the east. (Please see attached Exhibit - Location Map.)



The Site is situated in a prominent location in Downtown Tempe, just one block west of Mill Avenue on the north side of University Drive. An eclectic mix of uses surround the Site, including the Chase parking structure to the north and additional Chase and public parking along with commercial development across Ash Avenue to the east. South of the Site across University Drive is University Square Market, a mix of one and 2-story retail, restaurant, and office uses. Immediately west of the Site is the Southern Pacific railroad, with Lot 1 @ Farmer Arts District – Parcel 1, a proposed 13-story mixed-use development, adjacent to the railroad. (Please see attached Context Plan.)

The General Plan Projected Land Use designation for the Site is “Mixed-Use”, reflecting the urban nature of this Downtown Core area. The General Plan Projected Residential Density designation for the Site is “Mixed-Use High Density Urban Core (>65dua).” The project’s proposed 9-story building height and density range of 139.94 to 161.46 du/ac complies with the General Plan presenting a compatible face to its surroundings. (Please see attached Exhibits - General Plan Projected Land Use Map and - General Plan Projected Residential Density Map.)

The Site is zoned CC PAD for high-intensity mixed-use development. Zoning surrounding the Site includes mainly mixed-use and commercial zoning such as MU-4 PAD, CC PAD, CC, GID, and CSS. (Please see attached Exhibit - Zoning Map.)



Adjacency to Site	Zoning	Current Use	GP Projected Land Use	GP Projected Residential Density
SITE	CC PAD	Vacant	Mixed-Use	High Density-Urban Core (> 65 du/ac)
North	CC PAD	Parking Garage	Mixed-Use	High Density-Urban Core (> 65 du/ac)
Northeast	CC PAD	Parking Garage	Mixed-Use	High Density-Urban Core (> 65 du/ac)
East	CC PAD	Commercial	Mixed-Use	High Density-Urban Core (> 65 du/ac)
Southeast	CC	Commercial	Mixed-Use	Cultural Resource Area
South	CC & GID	Commercial	Mixed-Use	High Density (up to 65 du/ac) & Cultural Resource Area
Southwest	GID	Commercial	Mixed-Use	Medium to High Density (up to 25 du/ac)
West	GID	Railroad/Vacant	Mixed-Use	High Density (up to 65 du/ac)
Northwest	GID	Railroad/Vacant	Mixed-Use	High Density (up to 65 du/ac)

Table 1.1 – Surrounding Zoning, General Plan Designations, and Uses

C. OVERVIEW – THE FORUM REAL ESTATE GROUP DEVELOPMENT AND DESIGN TEAM

To bring this high-quality development to the City of Tempe, Forum and development partner Alberta Development Partners assembled a first-rate team of development professionals.

1) Forum Real Estate Group, LLC 

Founded in 2010, Forum Real Estate Group is a full-service, private equity real estate investment and development firm focused primarily on the multifamily sector. Forum prides itself on creating thoughtful, branded communities that residents call home.

Forum is a tight-knit group with deep expertise united by a passion for creating great experiences. The result is a highly personal, specialized and responsive approach that comes through in everything it does from development to design to property management.

Forum’s portfolio includes a variety of developments each designed to exceed expectations and deliver a unique, personalized branded experience for its residents. Forum’s development and management experience covers a broad spectrum of property types, including commercial business development and a range of multi-family products tailored to different levels of service.

The Foundry will become part of Forum’s “Signature Collection” - positioned in irreplaceable, marquee locations that represent Forum’s CORE/CORE+ multifamily assets. These well-appointed communities feature luxury, resort-style amenities and offer a personalized, branded “boutique hotel” experience.

2) Alberta Development Partners, LLC 

Alberta Development Partners is engaged in the acquisition, development and investment of retail and residential real estate opportunities located throughout the western United States. Alberta focuses its efforts on regional retail shopping centers, mixed-use commercial and residential projects, select build to suit opportunities that include grocery anchored shopping centers and value added opportunities. In the past four years, Alberta has closed on in excess of \$162 million worth of land for new development, delivered \$460 million worth of completed projects, and presently has \$812 million in active construction

projects, making Alberta one of the most aggressive retail and mixed-use development firms in the marketplace. Through extensive research, an appreciation for superior design and a passion for the details, Alberta creates lasting communities.

Alberta's principals have experience in all aspects of the business, from acquisition to design and construction to leasing, operations and management. Through the vision and leadership of their diverse team, they have completed in excess of 220 projects with a combined value of more than \$3.5 billion, becoming one of the most successful developers of exceptional retail and residential communities.

3) Humphreys & Partners Urban Architecture, LP



For over 24 years, Humphreys & Partners Architects has been providing high quality, innovative planning and design services. An award-winning firm specializing in multifamily, mixed-use, and hospitality/resort design, HPA has extensive experience in high-rise, mid-rise, student, senior, tax credit, affordable, moderate, and luxury communities.

Headquartered in Dallas, with regional offices all over the world, Humphreys & Partners Architects has provided design services on over 350,000 units to date in 49 different states. HPA employs a talented team of professionals with artistic talent, classical training, and diverse experience in all aspects of the architectural process.

4) Kimley-Horn

Kimley-Horn is one of the country's premier design consulting firms and provides a wide range of consulting services to both public and private clients. Kimley-Horn provides a full suite of land development services including predevelopment and regulatory approvals; site civil engineering, land planning and landscape architecture; environmental services; surveying and mapping; traffic engineering; and parking studies and design.

From site selection to initial design and on through construction, Kimley-Horn's civil engineers and land planners consult extensively with its traffic engineers, landscape architects, and environmental engineers to ensure a fully-integrated design.

Kimley-Horn has been honored repeatedly for its outstanding work and for its quality as a workplace.

5) PK Associates LLC



PK Associates provides a full range of structural engineering services from conceptual design to construction administration, with extensive experience in structural design, renovation, condition assessment analysis, and special structural inspections. PK Associates currently has offices throughout the southwest and has experience across thirteen different market sectors and is professionally registered in 46 states. PK Associates' design team is uniquely qualified with the necessary structural engineering experience, extensive knowledge of local conditions, a good working and personal relationship with local subcontractors, and knowledge of local construction practices.

6) Peterson Associates



PETERSON ASSOCIATES
Consulting Engineers & Architects

Peterson Associates is one of the Southwest's leading consulting engineering firms, specializing in the design of mechanical, plumbing, electrical, and fire protection systems, as well as construction administration services since 1982. Its experienced design staff includes professional engineers registered in 25 states and an experienced team of technical and administrative personnel.

Peterson Associates is dedicated to providing energy efficient and environmentally responsible designs, and is active in the LEED process and the employment of renewable energy technologies, conservation of natural resources, improved indoor air quality, lighting and building operations and maintenance.

Peterson Associates' portfolio includes commercial, healthcare, educational, industrial, public, and private projects for both regional and national clients.

7) Sender Associates, Chartered



The Law Offices of Sender Associates, Chartered is a woman-owned Zoning and Land Use law firm concentrating in mixed-use, commercial, industrial, and residential zoning entitlement matters. Sender Associates concentrates on assisting clients navigate the various political entitlement processes at the municipal and county levels throughout Arizona. The firm includes members with backgrounds in architecture, planning, and law who are highly-qualified to handle entitlement matters such as zoning changes, general plan amendments, lobbying, design review processing, neighborhood participation, and building permit processing, among many others. They also provide special development project support including zoning due diligence, vesting analysis, and litigation support.

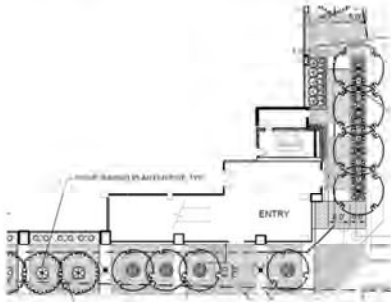
D. THE FOUNDRY DEVELOPMENT CONCEPT

The Foundry features true vertical integration of commercial and residential uses to activate the Site and bring the energetic environment from Mill Avenue to the edge of the Mill Avenue District. The mid-rise development is anchored by a 2-story volume Whole Foods that will provide a much needed grocery store to serve the Downtown Tempe area. Three levels of structured parking above the building base are incorporated into the building design through the use of an artistic screening system. Five levels of luxury apartments wrap around a resort-quality pool and resident amenity deck on the 5th floor.

The building features modern architectural design with timeless elements that incorporate seamlessly into the downtown fabric. The building massing features recesses and projections that create movement and play throughout the design. Clean lines throughout provide rhythm and define spaces.



An entrance vestibule for Whole Foods located at the corner of University and Ash provides pedestrian access from both streets. A storefront window system extends along both University Drive and Ash Avenue, providing visual access to the Whole Foods interior. Along University Drive, outdoor and mezzanine level dining patios connect the building to the street. The University Drive façade also features an 8th Floor clubhouse that cantilevers from the building, projecting over the right-of-way below. Ash Avenue serves as the residential front door, providing pedestrian access to the residential lobby, concierge, and leasing lounge.



The landscape plan is designed to enhance the streetscape and provide a comfortable pedestrian environment. Street trees along the right-of-way define the pedestrian path and create a continuous shade canopy. Variety in tree species will be provided adjacent to the outdoor dining area to distinguish the space and provide variation in both color and texture. Along the western property line, decomposed granite and vertical plantings will provide aesthetic interest and help prevent loitering in the area and graffiti on the building façade. The landscape plan continues on the 5th floor open-air amenity deck. A centrally located saltwater pool, lazy river, and hot tub are surrounded by various landscape plantings and outdoor seating areas.



Sample Forum Development resident amenity area

The Site is conveniently located within walking distance of a number of Tempe attractions and destinations. This makes The Foundry an ideal place to live, work, shop, or dine. Additionally, for Whole Foods it is an ideal location to serve a large and diverse consumer base. To capitalize on this location, The Foundry incorporates a number of design elements that create an inviting environment that is easily accessible.

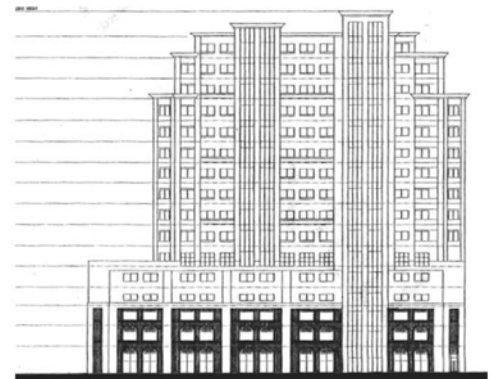
II. APPLICATION REQUESTS

A. PLANNED AREA DEVELOPMENT OVERLAY AMENDMENT

1) Entitlement History

a. 2005 Rezoning & Preliminary PAD Approval

On June 2, 2005, a 1.77 acre portion of the Site was rezoned from GID, General Industrial District (formerly I-2, General Industrial) to CC for the previously proposed 16-story mixed-use Cosmopolitan development. The high-rise mixed-use development concept included 175 residential condominiums, a grocery store, restaurant, additional commercial area, and below and above grade parking. The proposal included a preliminary PAD request establishing Site data including residential density at 98.9 dwelling units per acre (du/ac), building height at 207 feet, and lot coverage at 78% with 2% landscape area.



South Elevation 2005 PAD

The 2005 proposal included a 16-story, T-shaped residential tower stepped back along University Drive from a 6-story base. Ground level retail space, awnings and landscape, and a second story restaurant with balconies were proposed to promote vitality and a pedestrian friendly environment.

The 2005 proposal, while much larger in scale than The Foundry, was nonetheless similar in land use and compatibility. Planning staff comments for the 2005 approval included:

- *The General Plan projected residential density for this site is high density residential use with greater than 25 dwelling units per acre. Compact residences have limited private outdoor space, but have access to shared, or common open space for recreation. Proximity to amenities and configuration of residences encourages resident interaction. This level of intensity should promote a village environment with easy access to goods and services, business and recreation.*
- *This project complies with the projected residential density by the proposed zoning change to CC, City Center District, which has no standard for maximum allowable density.*
- *This project provides a mixed-use design, currently providing both residential and commercial uses.*
- *This site is within the University / Hayden Butte Redevelopment Area. The redevelopment areas encourage revitalization of adjacent areas with the intent to rebuild or redevelop, and encourage reinvestment. While the downtown has gone through a major transformation over the past twenty (20) years, this site has remained an Industrial zoned property prior to the area designated as a redevelopment area.*
- *The project is located within one of the General Plan's identified employment nodes. Providing additional residents within the downtown will ensure that this area can provide the community with a unique area of the city. The commercial base for the area would be able to support the appropriate level of good and services for the community, while at the same time providing a unique tourist attraction for visitors.*
- *The project will provide a balance of residential, commercial, and recreational uses, while creating a walkable community enhanced by mass transit.*
- *Providing a mixed-use project, the number of vehicle miles traveled will potentially be reduced, when providing an urban development.*
- *This proposal provides a diverse use of land through integration of residential and commercial spaces potentially limiting the amount of typical trips generated from residential. The Cosmo Building is located adjacent to downtown, which provides several options to restaurants, some services, recreation and entertainment. The downtown is a pedestrian environment offering walking distance to nearby businesses.*

Virtually all of these comments remain true today for The Foundry proposal. Often the passage of time could dilute some of these rationale, however, Downtown Tempe has only grown more mature since this 2005 approval. Many new buildings have been approved and developed. The skyline of Tempe from

all directions has begun to take shape just as the General Plan and the many area plans anticipated. The overall growth and maturation of Downtown Tempe supports development such as The Foundry, an important project that will provide housing options and long overdue downtown grocery store.

Planning Staff's reasons for approval of the 2006 Rezoning and PAD included:

ZON-2005-05

1. *The project meets the General Plan Land Use and Projected Residential Density for this site.*
2. *The project changes the zoning district from a General Industrial District to CC, City Center District. This zoning is compatible with other adjacent areas also zoned CC.*
3. *This site is considered part of the Downtown Tempe area.*

SPD-2005.32

1. *The project meets the General Plan Projected Land Use and Projected Residential Density for this site.*
2. *This site is considered part of the Downtown Tempe area.*
3. *The project will meet the development standards required under the Zoning and Development Code. The PAD Overlay provides the flexibility through establishment of the project specific standards. .*

Those reasons for approval are still true today:

1. *The Foundry meets the General Plan Land Use and Projected Residential Density for the new General Plan 2040.*
2. *The CC zoning remains in place today and continues to be compatible with the surrounding zoning and land uses.*
3. *The site remains part of the Downtown Tempe area, which continues to be home to a vibrant mix of uses, including many recently approved and developed projects.*
4. *The Foundry will establish project specific standards through the PAD Overlay, which remains a tool for flexibility in all development standards.*

b. 2006 Incorporation of Additional Property

On May 17, 2006, a ±0.16 acre portion of CC-zoned property was administratively removed from the Centerpoint PAD for inclusion in development of the Cosmopolitan project. This additional land remains part of the Site today.

c. 2006 Final PAD Approval



On July 6, 2006, the Cosmo preliminary PAD was modified by approval of a final PAD for the Site. The final PAD approval included a number of changes to the 2005 Preliminary PAD approval, including incorporation of the 0.16-acre Centerpoint property. The updated development proposal increased the number of residential units to 187 from 175, resetting the residential density at 96.8 du/ac. The building height was increased from 16 stories and 207 feet to 18 stories and 235 feet (247 feet including mechanical). The minimum landscape area was set at 2% and the lot coverage at 70%. Parking numbers

were set for its specific mix of uses, which included the residential units and 53,172 sqft of commercial area for a proposed grocery store. These are the PAD development standards currently in effect on the Site.

Similar in use and scale to The Foundry, the 2006 PAD was recommended for approval by Planning Staff, stating:

- *This proposal is consistent with the General Plan 2030 Projected Land Use and Residential Density maps. Staff recommends approval of the request subject to conditions.*
- *The project provides a mixed-use development of both commercial and residential living space, promoting the concept of live, work and recreate in one place.*
- *The proposed development achieves goals for in-fill development...*

Again, the statements supporting the 2006 PAD ring true today for The Foundry. Today's proposed development includes a similar mix of commercial and residential uses supported by General Plan 2040.

Planning Staff's reasons for approval of the 2006 PAD mirrored those in the 2005 Preliminary PAD approval:

1. *The project meets the General Plan Land Use and Projected Residential Density for this site.*
2. *The project will meet the development standards required under the Zoning and Development Code. The PAD Overlay provides the flexibility through establishment of the project specific standards.*
3. *The development provides a unique project that vertically integrates residential and commercial uses that promotes a village concept.*

And again, those reasons for approval remain in effect today:

1. *The Foundry meets the General Plan Land Use and Projected Residential Density for the new General Plan 2040.*
2. *The Foundry will establish project specific standards through the PAD Overlay, which remains a tool for flexibility in all development standards.*
3. *The Foundry proposes the same mix of uses and vertical integration that promote a village concept and made the prior Cosmopolitan proposals unique.*

Stalled by market conditions of the Great Recession, the Site changed hands and was ultimately purchased by Alberta Development. With an understanding of the past zoning and PAD approvals, the General Plan designations, and other planning documents, Alberta sought a developer that could complete the mixed-use vision for the Site in today's recovering market. After vetting a number of development firms, Alberta confirmed Forum as the developer best suited to complete the mixed-use project and ultimately purchase and manage the Site. Forum understands the importance of this Site and its development potential and looks forward to bringing this long-awaited development to the City of Tempe.

2) The Foundry Requested Modifications

Five development standards will be changed or set by the proposed Foundry development. Two changes will exceed the standards set by the current PAD. Two other development standards are within the parameters of the current PAD, but must be amended pursuant to the Zoning and Development code. The final amendment addresses parking, which must be amended to accommodate the proposed change in residential unit mix and commercial square footage.

a. Modifications to the Approved PAD

- i. Residential Units / Residential Density. Forum seeks to increase the overall number of residential units and residential density from the previously approved 187 units and 96.8 du/ac. The proposed increase in residential units and overall density is in response to current market conditions. Forum's plans include 293 units, however to accommodate the inevitable revision of unit mix during the construction process, Forum respectfully requests flexibility in the PAD approval to allow a range of proposed units between 260 and 300.

This unit range will establish a residential density of no less than 139.94 du/ac and no greater than 161.46 du/ac. By setting a proposed range of residential density, Forum will be able to finalize the construction details and adjust the unit mix within the building as needed without another PAD amendment to address a small change to the total number of units.

- ii. Parking. This PAD amendment will establish new standards for required parking based on the updated mix of uses specific to The Foundry. Under the newly-adopted CC District Parking Standards, a total of 261 vehicle parking spaces are required for the Site using 293 units as a base. The Foundry will include 564 parking spaces. While this is above the total required, this parking matrix is consistent with market standards for residential and grocery uses. The provided parking will ensure that parking demands are met for the maximum proposed number of residential units and any unforeseen shift in unit mix. The parking also ensures that adequate parking is available for Whole Foods patrons, especially during peak hours.

The parking is located on three levels of above-grade structured parking. Grocer parking will be located on the first garage level and secured resident parking will be located on the second and third garage levels. Parking provided in excess of the required parking will include compact and tandem parking spaces. Compact spaces will be located throughout all levels of parking for both commercial and residential use, while tandem spaces will be limited to residential parking.

- iii. Building Height. The building height will be reduced from the existing approved 247 feet to top of mechanical to no greater than 130 feet to top of mechanical. The 2005 PAD approved a building height of 16-stories and 207 feet. The 2006 PAD approval increased that building height to 18 stories and 235 feet. The Foundry building height will not exceed 130 feet. The current building design is 9-stories and 117'-4".

To once again accommodate potential shifts during the more detailed construction document design process, a maximum building height of 130 feet is proposed. This building height cap is significantly lower than the allowed 235 feet and is compatible with the surroundings.

- iv. **Lot Coverage.** The Foundry proposes to increase the maximum lot coverage from 70.2% to 92.27%. The increase in lot coverage is a result of a new building design and lower building height. To bring a full service grocer like Whole Foods to a downtown location requires maximizing retail floor space and parking for patrons. By expanding the building footprint, The Foundry design team was able to do just this while still providing a meaningful pedestrian ground floor experience.
- v. **Landscape Area.** The Foundry will also increase the minimum landscape area from 1.2% to 2.0%. According to the 2006 PAD, the Cosmo landscape area was set at 1.2%. The Foundry proposes to increase this landscape area to 2.0%. Due to the urban nature of the Site's location, the multitude of public utilities in the street-adjacent areas, and the Urban Streetcar right-of-way and station area requirements, quite a bit of the land surrounding the building must be hardscaped. Our landscape architect has worked with Planning Staff to ensure a pleasant and lush pedestrian experience.

b. PAD Proposed Development Standards Table

TABLE 4-203(B)	CC DEV. STANDARDS	SPD-2006.53 (COSMO AMENDED)	PAD PROVIDED (CC PAD)
GENERAL PLAN LAND USE	MIXED-USE	MIXED-USE	MIXED-USE
GENERAL PLAN DENSITY	HIGH DENSITY (>25 DUA)	HIGH DENSITY (>25 DUA)	HIGH DENSITY URBAN CORE (>65 DUA)
SITE AREA	-N/S-	84,070.84 SF (1.93 AC)^	80,938.00 SF (1.858 AC)
DWELLING QUANTITY	-N/S-	187 UNITS	293 UNITS (MIN 260, MAX 300 UNITS)
DENSITY	-N/S-	96.90 DUA	157.70 DUA (MIN 139.94, MAX 161.46 DUA)
BUILDING HEIGHT	50'-0"	247'-0"	117'-4" (MAX 130'-0") (TO TOP OF MECH)
BUILDING LOT COVERAGE	-N/S-	70% (58,849.56 SF/84,070.80 SF)	92.27% (74,680.00 SF/80,938.00 SF)
SITE LANDSCAPE COVERAGE	-N/S-	1.2% (1,008.85 SF/84,070.80 SF)	2.00% (1,619.00 SF/80,938.00 SF)
BUILDING SETBACKS			
FRONT (UNIVERSITY)	0'-0"	5'-0"	0'-0"
REAR (NORTH)	0'-0"	2'-0"	0'-0"
SIDE (WEST)	0'-0"	20'-0"	0'-0"
STREET SIDE (ASH)	0'-0"	5'-0"	0'-0"
VEHICLE PARKING QUANTITY	(TABLE 4-603(E))	(TABLE 4-603(E))	(CC STANDARDS ORD. O2015.60)
GROCER	136.70 (41,010 / 300)	177 (53,172 SF / 300)	72.02 (41,010-5,000 / 500)
ACCESSORY RESTAURANT	16.00 (1,200 / 75)	0.00	0.00 (1,200-5,000 / 500)
ACCESSORY OUTDOOR PATIO	6.00 (900 / 150)	0.00	0.00 (900 / 0.00)
1-BEDROOM RESIDENTIAL	399.00 (266 * 1.5/UNIT)	170 (113 * 1.5/UNIT)	133.00 (266 BR * 0.5/BR)
2-BEDROOM RESIDENTIAL	44.00 (22 * 2.0/UNIT)	112 (56 * 2.0/UNIT)	22.00 (44 BR * 0.5/BR)

3-BEDROOM RESIDENTIAL	12.50 (5 * 2.5/UNIT)	45 (18 * 2.5/UNIT)	4.50 (15 BR * 0.3/BR)
GUEST RESIDENTIAL	58.60 (293 * 0.2/UNIT)	37 (187 * 0.2/UNIT)	29.30 (293 * 0.1/UNIT)
PARKING REQUIRED:	673 SPACES	541 SPACES	261 SPACES
PARKING PROVIDED:		646 SPACES	564 SPACES
BICYCLE PARKING QUANTITY			
GROCER	5.47 (41,010 / 7,500) (MIN 4)	13 (53,172 SF / 7,500 SF)*	5.47 (41,010 / 7,500) (MIN 4)
ACCESSORY RESTAURANT	2.40 (1,200 / 500)	0.00	2.40 (1,200 / 500)
ACCESSORY OUTDOOR PATIO	0.45 (900 / 2,000)	0.00	0.45 (900 / 2,000)
1-BEDROOM RESIDENTIAL	199.50 (266 * 0.75/UNIT)	85 (113 * 0.75/UNIT)	199.50 (266 * 0.75/UNIT)
2-BEDROOM RESIDENTIAL	16.50 (22 * 0.75/UNIT)	42 (56 * 0.75/UNIT)	16.50 (22 * 0.75/UNIT)
3-BEDROOM RESIDENTIAL	5.00 (5 * 1.0/UNIT)	18 (18 * 1.0/UNIT)	5.00 (5 * 1.0/UNIT)
GUEST RESIDENTIAL	58.60 (293 * 0.2/UNIT)	37 (187 * 0.2/UNIT)	58.60 (293 * 0.2/UNIT)
PARKING REQUIRED:	288 SPACES	195 SPACES	288 SPACES
PARKING PROVIDED:		262 SPACES	288 SPACES
USES & AREAS			
TOTAL GROSS BUILDING AREA		733,419 SF	570,609 SF
GROCER (INCL. MEZZ.)		53,172 SF	41,010 SF
LOBBY/LEASING/AMENITIES		-UNK-	10,086 SF
CLUBHOUSE		-UNK-	6,837 SF
LOADING & TRASH		-UNK-	1,138 SF
MEP/STORAGE		-UNK-	7,934 SF
MAJOR VERT. PENETRATIONS		-UNK-	9,142 SF
PARKING		-UNK-	249,366 SF
GENERAL COMMERCIAL		53,172 SF	0.00 SF
GROSS RESIDENTIAL AREA		385,653 SF	245,096 SF
NET RESIDENTIAL AREA		-UNK-	210,395 SF
USE PERMITS			
TANDEM PARKING		-N/A-	19 TANDEM STALLS

Table 2.1 – PAD Development Standards Table

3) City of Tempe PAD approval criteria

- a. The development fulfills certain goals and objectives in the General Plan, and the principles and guidelines of other area policy plans. Performance considerations are established to fulfill those objectives.

As discussed in more detail below, the proposed development fulfills many of the goals and objectives found in the General Plan as well as principles and guidelines of the Community Design Principles – Downtown/Mill Avenue District, Mill & Lake District Streetscape Principles and Guidelines, and the Urban Open Space Plan. The PAD development standards are set to maximize the Site’s development potential in line with the goals and objectives of those plans.

i. Community Design Principles - Downtown/Mill Avenue District

The development meets the vision and many of the principles set forth in the Community Design

Principles Downtown/Mill Avenue District, a community design plan accepted by the City Council's Central City Development Committee of the Whole on April 6, 2006. (Please see attached Exhibit - Downtown/Mill Avenue District Map.) The plan was adopted to ensure that the area's essential qualities and distinctive characteristics were included as a point of reference for envisioning future development. The goal was not to set static standards but rather guide redevelopment to achieve a high-quality environment with a special sense of place. The plan identifies four "cornerstone" principles – integration, innovation, articulation, and definition – as concepts that should be incorporated into development. The plan also includes guidelines for building heights in the Downtown/Mill Avenue District. The Foundry implements the cornerstone principles of the plan and proposed building heights are well within the ranges set by the plan.

(a.) General Principles

Many of the principles indicated in the Community Design Principles are applicable to The Foundry.

- Limited in horizontal expansion by finite municipal boundaries, the future of Tempe's built environment will be determined not by planned sprawl, but by principled infill, redevelopment, rehabilitation and preservation.

The Foundry is a principled infill development that responds to Tempe's needs for the future. As Tempe continues to grow, providing housing options and services is critical to support continued economic growth for the city. The Foundry will bring housing options and a much needed grocery store to a currently vacant lot in the Downtown area, filling a growing need for the city.



- Pedestrian realm. Street level activity, in which automobiles participate, but not dominate, is essential to an attractive, vital community.

Pedestrian activity at the Site is a key component of making The Foundry a successful development. The Site is located west of the vibrant pedestrian realm of Mill Avenue, and as such faces the challenge of extending that pedestrian activity west of Mill Avenue. To continue the active environment established on Mill at the Site, the design is inviting, comfortable, and lively. The Foundry includes a Whole Foods grocery store, which will serve the entire Downtown Area, including the vibrant Mill District, the developing Farmer Arts District, and nearby established residential neighborhoods including Maple-Ash, Sunset, and Riverside.

- This "eclectic urban oasis" is both a destination and home. Future [re]development contributes to its success through the well-considered, creative design of its structures and spaces.

Forum respects the dynamic variety of existing development in Downtown Tempe as well as the history that made the core the "eclectic urban oasis" it is today. With that in mind, the design team spent a significant amount of time learning the culture and history of the Site and surrounding area to guide both the architecture and interior design of the building. The result is a structure that borrows elements from its surroundings to reflect the community with design and details that establish its own unique identity as a destination in Downtown Tempe.

(b.) Four “Cornerstone” Principles

The Foundry incorporates many aspects of integration, innovation, definition, and articulation – community, character, streets/spaces, function, planning, urban design, architecture, and visualization. The Foundry enhances the pedestrian experience along University Drive and Ash Avenue to maintain a human-scale environment. The ground level along University drive includes large, roll-up windows to increase permeability, while interactive encroachments such as sidewalk dining areas, balconies, and building entries address the street to provide access and define function.

The building design incorporates new and traditional materials honoring the downtown context while establishing its own style and unique identity for the Site. Public art will be incorporated into the garage screening to enhance the building design and blend seamlessly with the overall architectural style.

(c.) Building Heights

The concept study for appropriate downtown building heights identifies and categorizes a range of building heights appropriate for the downtown area. The Site is within the “Urban Center” building height category appropriate for buildings up to 300 feet. (Please see attached Exhibit - Downtown Building Heights Concept Study.)



At 117'-4" (max. 130'-0"), The Foundry falls well within the building height range supported by this Study. As in 2005 and 2006, the Concept Study remains effective today and still supports building heights on the Site up to 300 feet.

ii. Mill & Lake District Streetscape Principles and Guidelines

The Mill & Lake District Streetscape Principles and Guidelines were adopted in February of 2011 to guide future development and encourage maintenance within the public rights-of-way in the Downtown Tempe/Mill + Lake District. (Please see attached Exhibit - Mill & Lake District Map.) The plan is founded on the understanding that growth and economic development cycles change and buildings come and go, yet the streets serve as a constant connecting fabric. From that understanding, the plan sets forth a variety of objectives - to create safe, comfortable, walkable environments that promote Tempe as a diverse community. The plan encourages the use of building and landscape design and materials that create an identity and encourage interactions at the street/pedestrian level. These concepts have been embraced by The Foundry design team and will continue to guide their decision making processes.

The Streetscape Plan includes guidelines for streetscape elements including building facades, street furniture, utility structures, landscape and elements, lighting, and sidewalks, and identifies street types and characters in the Downtown Tempe/Mill + Lake District. One of the first Guidelines in the Plan refers to Building Facades:

Emphasize ground floor building facades with awnings and overhangs which serve to “embrace” pedestrians, transparent windows with articulated entries to achieve “permeability” and interesting architectural details that catch the eye, project a comfortable human scale and engage the passer-by.

The Foundry embraces the human scale at the ground level. The storefront window system along University and Ash provides visibility into the grocery store, engaging the passer-by. The window system is broken up by brick and metal to add visual interest. Along University Drive, the façade features large, roll-up doors that open to the outdoor dining area, with mezzanine dining above. This feature opens the building, enhances the permeability of the structure, and provides opportunities for engagement in the pedestrian realm. Along Ash Avenue, awnings project over the sidewalk, providing shade and creating a comfortable pedestrian environment.

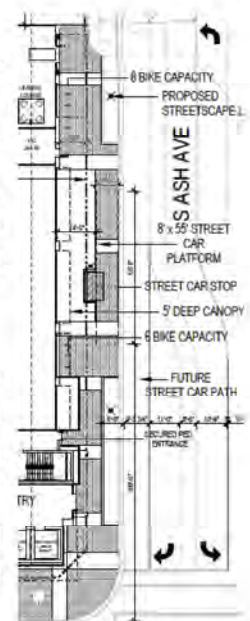
The plan calls for landscaping that considers the growing conditions of an urban hardscape environment. Additional color and form should be provided through planting desert shrubs.

The Foundry faces unique challenges with landscape planning along University Drive due to an existing irrigation line and public utilities. The design team continues to work with city staff to provide the appropriate trees in planters that are best suited to meet the goals of maintaining rhythm and continuity while providing adequate canopy for shade and comfort. Along Ash Avenue a complementary species of tree will be planted in the ground. The landscape plan is completed with a variety of desert shrubs, groundcover, and hardscape materials

The Streetscape Plan identifies University Drive as a “Town + Gown/Campus Transition” street. University Drive is a Major Arterial street that carries a high volume of traffic, both separating and linking Downtown to ASU, and has its own Comprehensive Master Plan. With the recent completion of the University Drive Streetscape Project, Forum respects the importance of development along this street and the impact on University Drive. The Foundry’s design along University Drive is intended to meet the same goals – promote neighborhood stability, sustainable business, and ensure that University Drive is a safe environment. A significant amount of time was spent by Forum’s development team to ensure that access to and from the Site on University Drive was safe. Creating a comfortable and vibrant pedestrian realm along University Drive is also an important component of The Foundry design. Placing the grocery store entrance at the corner and dining areas along University is intended to help activate the streetscape.

Ash Avenue is identified as an “External Connector/Transit” street meant to connect the internal core of the district with adjoining neighborhoods or other connectors, including current or planned transit routes. The section of Ash Avenue adjacent to the Site is the proposed location for the future Urban Streetcar line, including a stop adjacent to the Site. Forum’s design and engineering team continue to work with the City to accommodate this public amenity and provide a comfortable environment for transit riders at this stop.

The Foundry design team has and will continue to refer to the streetscape principles and guidelines as well as Staff input in finalizing the streetscape designs for the project. An area of significant importance is continued work with staff to accommodate the future streetcar alignment and stop. The streetscape design will include street furniture, landscape and hardscape, planters and planting areas, and lighting to create a comprehensive streetscape that enhances the quality of the district.



iii. Urban Open Space Plan

Adopted in 2007, the Urban Open Space Plan is the result of more than a year's effort to develop a vision for downtown Tempe's public spaces and build on Tempe's unique downtown experience. The plan explores how the city could develop a great public space environment that attracts residents, businesses, and students for decades to come. The plan includes strategies to implement and operationalize a system of great, active public spaces, including recommendations for the city to manage, maintain, and finance them.

To develop a vision for downtown's public spaces, downtown is divided into different planning elements – anchors, building blocks, and neighborhood places. The Site is located in the Mill Avenue District anchor area and Sunset Riverside neighborhood place and is adjacent to the Southwest Neighborhoods neighborhood place. The plan considers “anchors” regional amenities that will serve the community for decades to come, while “neighborhood places” are evaluated in the plan to look at opportunities from the point of view of downtown's residents.

Mill Avenue is one of the most recognizable and important areas for investment in the public realm. Through assistance of the Downtown Tempe Community (DTC – now known as the Downtown Tempe Authority) the dynamic of Mill Avenue has extended into a larger area – the Mill Avenue District. A key focus of the Mill Avenue District anchor area is expanding the experience of Mill Avenue to the rest of downtown. By implementing conditions along Mill that contribute to the vibrant atmosphere – streetscape, outdoor dining, and a human scale – The Foundry will extend the activity of Mill Avenue to the Site.

The Maple-Ash, Sunset, and Riverside neighborhood areas are among Tempe's most distinctive and historic areas and enjoy proximity to Town Lake, ASU, and the Mill Avenue District. While participating in the Open Space Plan creation, residents of those neighborhood areas expressed challenges facing the areas and hopes for the future. University Drive was identified in both areas as a challenge for creating a safer and more pedestrian friendly environment. Development of the Site will help create the pedestrian friendly environment along University that residents of the adjacent neighborhoods are asking for.

By replacing a vacant lot that has no streetscape improvements with a vibrant, mixed-use development, the Site will be a more welcoming and active environment. In addition to creating a comfortable streetscape environment through the landscape design, The Foundry is working with the City to design for the future Streetcar line and stop at the Site. The results will be a significant improvement from today's conditions along both University and Ash along the Site, creating a safe and pedestrian friendly environment.

iv. General Plan Goals and Objectives

(a.) Land Use Element and Projected Land Use

Goal: *Foster quality development through land use that provides sustainable growth and enhances the quality of life where people live, learn, work and play.*

The Foundry provides infill redevelopment of a Downtown Core site that will promote Tempe’s vision for a quality environment where people live, learn, work, and play. The development will support Tempe’s significant financial investments in Light Rail, Tempe Town Lake, the Downtown, and the proposed Modern Streetcar by providing a mix of uses and the residential density needed to support these investments. The project meets the General Plan’s projected land use (Mixed-Use) and residential density (High-Density Urban Core >65 du/ac) designations on the Site and achieves a compact, efficient infill redevelopment of a vacant, underutilized site.

As Tempe sees continued success in its Downtown Core development with major projects such as Marina Heights and Hayden Ferry Lakeside III under construction, The Foundry will add to the dynamic environment by providing high-quality resort-style living and retail uses in the city’s urban core. The development will support Tempe’s growing economy as employees brought in by the city’s premier commercial development areas including Tempe Town Lake, Discovery Business Park, ASU Research Park, Marina Heights, and Liberty Center at Rio Salado look for housing, shopping and entertainment options in the city.

(b.) Community Design Element

Goal: *Community Design - Promote design and development standards that improve the community’s visual quality, urban form, and functionality to enhance the quality of life for future generations.*

The Foundry meets the goals and objectives of the Community Design Element by incorporating the design principles necessary to create an accessible environment with a sense of place. Accessibility is maximized by using design elements and articulation to create a human-scaled environment along the street frontages. The ground level is activated by Whole Foods, creating a project that is human-scaled and accessible to the public.

The Site is a key link between Downtown Tempe and the surrounding community. Landscape and hardscape that reflect the Site’s context will extend the architectural experience of the building to the pedestrian realm. By improving the streetscape surrounding The Foundry to make it a more comfortable environment to navigate, the connections between the Site and surrounding areas will be maintained and strengthened.

Most importantly, the ground level grocer retail emphasizes that connectivity drawing pedestrians to the area.

(c.) Redevelopment Element

Goal: *Sustain or maximize the efficiency of land uses within areas of stagnation or decline by promoting the greatest economic, social and cultural potential.*



The Site has been underutilized and vacant for many years and is in need of redevelopment to re-activate this important Downtown parcel. The Foundry will maximize the potential of the Site by including a diverse mix of uses, putting the Site to its highest and best use pursuant to the General Plan 2040. Development of The Foundry will bring a much desired neighborhood grocer and high-quality residential development to the Downtown area. The Foundry will also stimulate

investment and development of other, underutilized property in the area. The Foundry will revitalize, re-activate and engage the neighborhood, making it distinct and identifiable.

(d.) Housing Element

Goal: *Provide diverse housing opportunities for current and future residents, for all income levels and household types, with specific focus on providing affordable housing to help those in greatest need.*

To ensure that Tempe's housing portfolio remains diverse and attractive to current and future residents and businesses, urban housing options must be added to the Downtown area. The Foundry will add diversity to existing Downtown housing options providing amenity-rich, luxury rental housing. The resort-style, high-end residential component will fill market demand that continues to rise as Tempe attracts more and more businesses to its growing economic centers.

(e.) Economic Development Element

Goal: *Stimulate a sustainable, diversified and vibrant economy and job force.*

The Foundry will be an economic driver for Tempe, contributing to the financial, economic, and cultural diversity of the community. The development will add retail jobs, residential units, and construction, all of which will contribute to the city's tax base. The retail, housing options and lifestyle provided in the development will help attract and maintain a highly trained workforce looking for options about where to live, work, learn, and play. This translates into increased quality of life for all residents of Tempe.

With the inclusion of Whole Foods, The Foundry will serve as an economic magnet for the Downtown area and will provide much needed support for all existing and future residential and commercial development in the Downtown area. With its diverse mix of retail and residential uses, The Foundry will ensure this high growth Downtown area reaches its full economic development potential.



(f.) Growth Area Element

Goals: *Rail Corridor Growth Area*

- *Attract mixed-use development along the rail corridor and create a dynamic and eclectic urban environment to maximize public investment hub of the region.*
- *Continue investment in improvements to make the Downtown a financial, civic, cultural and professional hub of the region*
- *Encourage reinvestment and establish compact, desirable and walkable urban neighborhoods*

Objectives:

- Focus mixed use development near rail investments to support reduced transportation costs for residents, and in turn, produce increased transit ridership.
- Create a walkable community enhanced by rail transit
- Encourage land use and development that creates a sense of community and place
- Sustain the Mill Avenue District as a regional destination
- Create a balance of residential, commercial, recreational and educational uses

- Create a unique urban environment and authentic image
- Encourage investment that builds a strong sense of community
- Continue investment consistent with the public investment in transit and reflective of transit-oriented design and development
- Enhance area quality of life for existing and future residents
- Promote desirable reuse of land

The Site is located in both the Light-Rail Corridor Growth Area and the NW Tempe Neighborhoods Area, making it a critical location for re-investment and redevelopment. The Site is uniquely situated, and given its mixed-use nature, The Foundry will serve as a key connection point between Downtown Tempe and the Tempe Neighborhoods to the west.

The Foundry has the potential to expand the urban activity centered on Mill Avenue and bring that vibrancy to the Site and surrounding area. The Site's location between transit, the Mill Avenue District, ASU and the NW Tempe Neighborhoods Area, the area around the Site experiences a significant amount of pedestrian traffic. By redeveloping the Site with active and engaging uses, the Site and surrounding area become a destination and place for engagement with its own unique image rather than an ignored part of a path connecting two other destinations. This type of creation and place-making is essential to redevelopment and growth.

(g.) Pedestrian Network, Bikeways, and Transit Element

Goal: *Pedestrian Network – Develop safe, comfortable walking environments and pedestrian connections to encourage pedestrian travel.*

Goal: *Bikeways – Expand and enhance bicycle travel within the City.*

Goal: *Transit - Coordinate and produce efficient, safe, convenient and interconnected transit options to increase ridership.*

Tempe's ability to meet future travel demands requires community support for transit systems. Increased ridership contributes to funding the system. To support Tempe's investment in the transit system, the areas along light rail – especially Downtown Tempe – must be developed with diverse uses and proper density.

The Foundry will bring that mix of uses and density to an area of Tempe that is highly connected with transit options. This influx of activity and people on the Site will help increase ridership on transit and support Tempe's investment in the transit system. Increased density in the Downtown catalyzed by The Foundry will also help cement federal funding for the Modern Streetcar.

(h.) Travelways Element

Goal: *Travelways Network - Encourage redevelopment of the street network that balances the needs for various types of travelers and more fully serves all modes of transportation safely and efficiently.*

Goal: *Parking and Access Management – Incorporate parking and access management strategies that influence travel behavior and reduce congestion on busy streets.*

Objectives:

- Establish guidelines that enhance land use and transportation connection
- Promote consolidated and shared use parking areas

To achieve the goals and objectives of the Travelways Element, development must facilitate pedestrian and bicycle usage. The design of The Foundry is highly pedestrian-oriented and incorporates the principles and guidelines identified in the Mill and Lake District Streetscape plan. Placing great emphasis on the pedestrian and bicycle elements ensures the project promotes a balance of all modes of transportation available to the Site including bicycles, vehicles, bus, and walking.

The development provides the proper balance of necessary parking for an urban mixed-use development based on a thorough parking study and analysis of market demands.

b. Standards requested through the PAD overlay district shall take into consideration the location and context for the Site for which the project is proposed.

The standards requested through the PAD are appropriate for the Site's location in Tempe's urban core. The requested PAD standards are performance based and unique to the project and Site, ensuring the development seamlessly integrates with the surrounding community. The Site's prominent location is within walking distance of a number of Tempe attractions and destinations. This makes The Foundry ideally situated for infill development utilizing the requested PAD standards.

Previously approved entitlements for the Site envisioned high-rise development with a similar mix of uses. The height requested in this PAD modification is significantly lower than the previous approvals and better reflects the surrounding context. The lower building height presents a better transition for the residential neighborhoods to the south and west.

The proposed residential density is greater than the previous approvals, but conforms with the Projected Residential Density of General Plan 2040. The requested density is similar to other downtown urban living found in proximity to the Site.

Parking, lot coverage, and landscape area standards proposed by the PAD are specifically tailored for The Foundry development. Urban infill projects require the flexibility in design that is accommodated through the proposed PAD standards. Adjusting the standards to accommodate the project ensures that creative design solutions are available to provide a high-quality and successful development that is compatible with the surroundings.

c. The development appropriately mitigates transitional impacts on the immediate surroundings.

The Foundry's design is compatible with the immediate surroundings and the requested PAD standards will ensure that impacts on the surroundings are appropriately mitigated. The Foundry is substantially shorter than prior approvals for this Site and matches the scale of nearby structures. The building is set closer to the property line at the ground level to activate the pedestrian realm. The building's architecture is aesthetically pleasing and will enhance the surrounding area. Mature street trees will add color and texture to the Site, creating a more visually pleasing and physically comfortable environment.

Elimination of the vacant lot will help complete the streetscape along University Drive, establishing connectivity to the surroundings.

Additionally, the proposed mix of uses and intensity is appropriate for the area. Additional housing options are necessary to ensure that Tempe retains residents seeking an urban living option and to capture new residents brought to the market by Tempe's growing business developments and economic base. Adding residents within walking distance to a number of key destinations and transit opportunities activates the area without burdening the traffic patterns. Whole Foods brings a much needed service to residents and employees of downtown Tempe.

B. DEVELOPMENT PLAN REVIEW

1) City DPR Approval Criteria

- a. Placement, form, and articulation of buildings and structures provide variety in the streetscape.

The Foundry's design features building facades that are designed with variation in materials, colors fenestration, and wall plans to provide variety and visual appeal. Clear visual elements along both street frontages provide an engaging environment at the ground level and distinguish the retail from the parking and residential above.

- b. Building design and orientation, together with landscape, combine to mitigate heat gain/retention while providing shade for energy conservation and human comfort.

The building is designed to mitigate heat gain by placement of open aired parking garage below residential units combined with limited openings along the west wall and green screening. A light colored heat resistant roofing system will be used that will also reduce the heat island effect. An open air courtyard brings natural light into the interior of the building. Energy efficient windows and doors and energy star appliances along with the use of native low water absorbing landscape all contribute to energy and water conservation.

- c. Materials are of a superior quality, providing detail appropriate with their location and function while complementing the surroundings.

High-quality materials are composed to create interest and a sense of place while maintaining a contextual feel with the surrounding environment. A variety of materials, including brick, metal screening, walnut textured laminate, metal paneling, and stucco define the spaces and add articulation to the elevations. The use of light colored stucco at residential levels and brick with metal canopies to provide shade at the pedestrian street level all are timeless design features that function well with the location. Unique screening for the garage levels will provide an artistic signature design for the building setting it apart from other developments.

- d. Buildings, structures, and landscape elements are appropriately scaled, relative to the site and surroundings.

The building is scaled appropriately to fit the Site and surroundings. The building height is appropriately scaled down from the taller structures further east and matches the heights of the nearby Chase parking structure, office development, and pending Farmer Arts Lot 1 development. The building extends closer to the property line and utilizes a clear, store-front window system to provide engagement at the street level. Landscaping elements create a comfortable, human scale and tie the building to the surrounding context.

- e. Large building masses are sufficiently articulated so as to relieve monotony and create a sense of movement, resulting in a well-defined base and top, featuring an enhanced pedestrian experience at and near street level.

Building masses are broken down into elements that invoke a sense of movement or play along the massing, creating a well-defined base that activates the pedestrian experience at street level. Whole Foods serves as the project base and creates a comfortable, human-scale environment that engages users in the Site. The pedestrian experience is enhanced with landscape and hardscape elements that define the outdoor spaces. Architectural canopies and shade trees add to the comfort of the pedestrian realm.

- f. Building facades provide architectural detail and interest overall with visibility at street level (in particular, special treatment of windows, entries and walkways with particular attention to proportionality, scale, materials, rhythm, etc.) while responding to varying climatic and contextual conditions.

At the street level the building façade is divided into a series of glass bays punctuated with brick columns that provide rhythm and bring the scale down to a human level. Overhead canopies along both street frontages provide shade and contribute to the proportionality of the buildings base. Whole Foods will have large glazed openings that will also contribute to street level visibility and create an active pedestrian experience. An entry vestibule anchored at the corner of University Drive and Ash Avenue provides pedestrian access from both streets.

- g. Plans take into account pleasant and convenient access to multi-modal transportation options and support the potential for transit patronage.

The Site has immediate and convenient access to multi-modal transportation options that connect the Site with destinations throughout Tempe and the greater Valley Metro area. Valley Metro local route 30 runs along University with multiple stops on University within proximity to the Site. Access to Valley Metro routes 65 and 66 is just east of the Site at Mill Avenue, and access to routes 62 and 48 as well as Orbit's Venus route is just north of the Site at Ash and 5th Street. The pedestrian-oriented nature of the development takes advantage of these nearby transit connections. Additionally, the future Tempe Modern Streetcar route runs along Ash Avenue with a stop planned adjacent to the Site. In anticipation of the future Streetcar stop, the design team has worked with staff to plan an accommodating pedestrian realm along Ash Avenue.

- h. Vehicular circulation is designed to minimize conflicts with pedestrian access and circulation, and with surrounding residential uses.

Forum's design and engineering team invested a significant amount of time researching and designing the best vehicular access and circulation for the Site to provide safe access that minimizes conflicts with pedestrians and existing traffic patterns as well as proposed building functionality. Primary vehicular access to the Site is via garage access on Ash Avenue, with secondary garage access on University Drive. The Ash Avenue access point is placed to the north side of the building, away from the pedestrian access for the grocer and residential leasing office to minimize conflicts with pedestrians. The University Drive access point is located to the west of the building, again far from any pedestrian access points to avoid conflicts.

- i. Plans appropriately integrate Crime Prevention Through Environmental Design principles, such as territoriality, natural surveillance, access control, activity support, and maintenance.

The Foundry design appropriately incorporates CPTED principles to promote safety and suppress crime. Opportunities for natural surveillance are created throughout the design to establish "eyes on the street" throughout the project. Gathering spaces, balconies, windows, and lighting throughout the Site increase visual surveillance of the Site. Clear distinction between public and private areas within the development will increase access control. Additionally, residential parking will have secured vehicle and pedestrian access controls. All access points will be well lit and visible to increase safety. Landscaping and building lighting will help deter loitering and vandalism along the western property line adjacent to the railroad.

- j. Landscape accents and provides delineation from parking, buildings, driveways and pathways.

The landscape plan includes street trees, desert shrubs, and hardscape materials arranged to enhance the streetscape and provide a comfortable pedestrian environment. Street trees along the right-of-way define the pedestrian path and create a continuous shade canopy. Variety in tree species will be provided adjacent to the outdoor dining area to distinguish the space and provide variation in both color and texture. Along the western property line, decomposed granite and vertical plantings will provide aesthetic interest and help prevent loitering in the area and graffiti on the building façade.

- k. Signs have design, scale, proportion, location and color compatible with the design, colors, orientation and materials of the building or site on which they are located.

Building and tenant signage will follow a comprehensive sign criteria established for the project. Whole Foods and The Foundry residential will each have address and identification signs. Sign materials will complement the building and any other signs on the Site. All sign areas and copy will be proportional to the size of the building element on which it is located.

- l. Lighting is compatible with the proposed buildings and adjoining buildings and uses, and does not create negative effects.

Proposed lighting will be compatible with the building and not create negative effects. The building base will be well-lit for safety and to provide visual cues. Architectural lighting will identify sign entrance points for the different building functions. Exterior building lighting will be a combination of LED recessed lights, wall sconces, and cut-off wall packs with light levels that meet the City's lumens standards.

C. USE PERMIT TO ALLOW TANDEM PARKING

Nineteen (19) tandem parking stalls are proposed for the residential parking on levels 2 and 3, for a total of 38 parking spaces. The proposed tandem parking is in excess of the required residential parking. Tandem parking requires approval of a use permit. The following factors shall be considered in approval of a use permit:

1) City Use Permit Approval Criteria

a. Any Significant increase in vehicular or pedestrian traffic.

The proposed tandem parking will not cause an increase in vehicular or pedestrian traffic. The amount of traffic, both vehicular and pedestrian, coming to and from the Site is impacted by a number of other factors such as the proposed use, accessibility to mass transit, and distance to and from other destinations, not by the provision of tandem parking spaces. The parking configuration is internal to the Site with the tandem spaces located on levels 2 and 3 in the designated residential parking areas. Use of tandem spaces will allow the development to maximize the parking available for the residential component of the project. Because the tandem parking is designated for residential units only, there will be no impact on parking for the commercial component.

b. Nuisance arising from the emission of odor, dust, gas, noise, vibration, smoke, heat, or glare at a level exceeding that of ambient conditions.

There will be no nuisance arising from the emission of odor, dust, gas, noise, vibration, smoke, heat, or glare at a level exceeding that of ambient conditions. The Site is adjacent to a railroad and located in an urban area that includes a variety of uses, arterial streets, and mass transit. The proposed tandem parking will be located within the parking structure for the development. The structure is surrounded by commercial and retail uses at the pedestrian level and screened with architectural materials beginning on the second level, serving as a buffer between the parking structure and surrounding uses.

c. Contribution to the deterioration or to the downgrading of property values which, is in conflict with the goals, objectives or policies for rehabilitation, redevelopment or conservation as set forth in the city's adopted plans or General Plan.

The proposed tandem parking will not contribute to the deterioration of the neighborhood or to the downgrading of property values. The parking is integrated in structured parking that is incorporated into the overall building design and will have no impact on property values. The tandem parking will provide flexibility in the garage design to maximize residential parking opportunities for the development.

d. Compatibility with existing surrounding structures and uses.

The tandem parking will be included in the proposed parking structure and screened from the surrounding structures and uses. The parking garage is an essential component of the development that will exist whether or not tandem parking is included. The tandem parking configuration is limited to two parking levels within the structure and will not be visible from the surrounding structures and uses.

- e. Adequate control of disruptive behavior both inside and outside the premises, which may create a nuisance to the surrounding area or general public.**

Tandem parking will not lead to disruptive behavior either inside or outside the premises.

III. CONCLUSION

For the reasons discussed above, Forum and its development partner Alberta respectfully requests approval of these entitlements. Forum looks forward to bringing the City of Tempe an exciting project that adds housing stock and provides a desired community asset to the Downtown area.

EXHIBIT - LOCATION MAPS

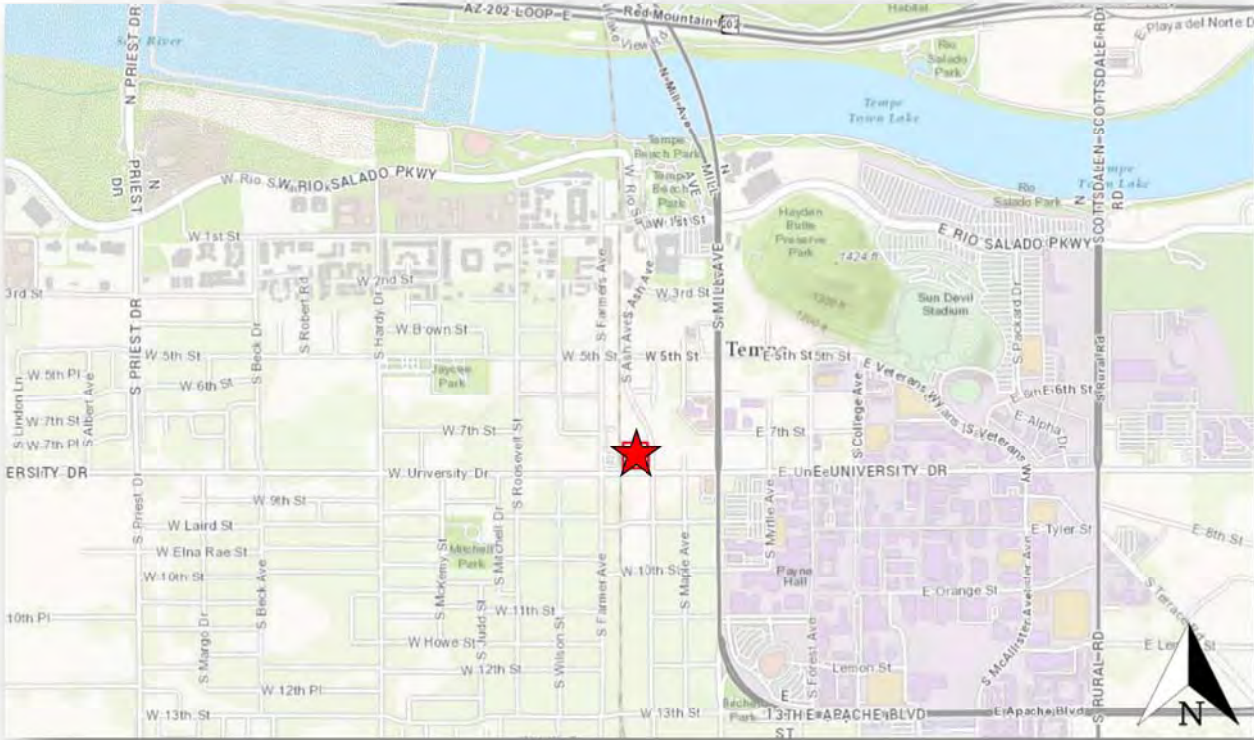




EXHIBIT - GENERAL PLAN 2040
PROJECTED LAND USE MAP

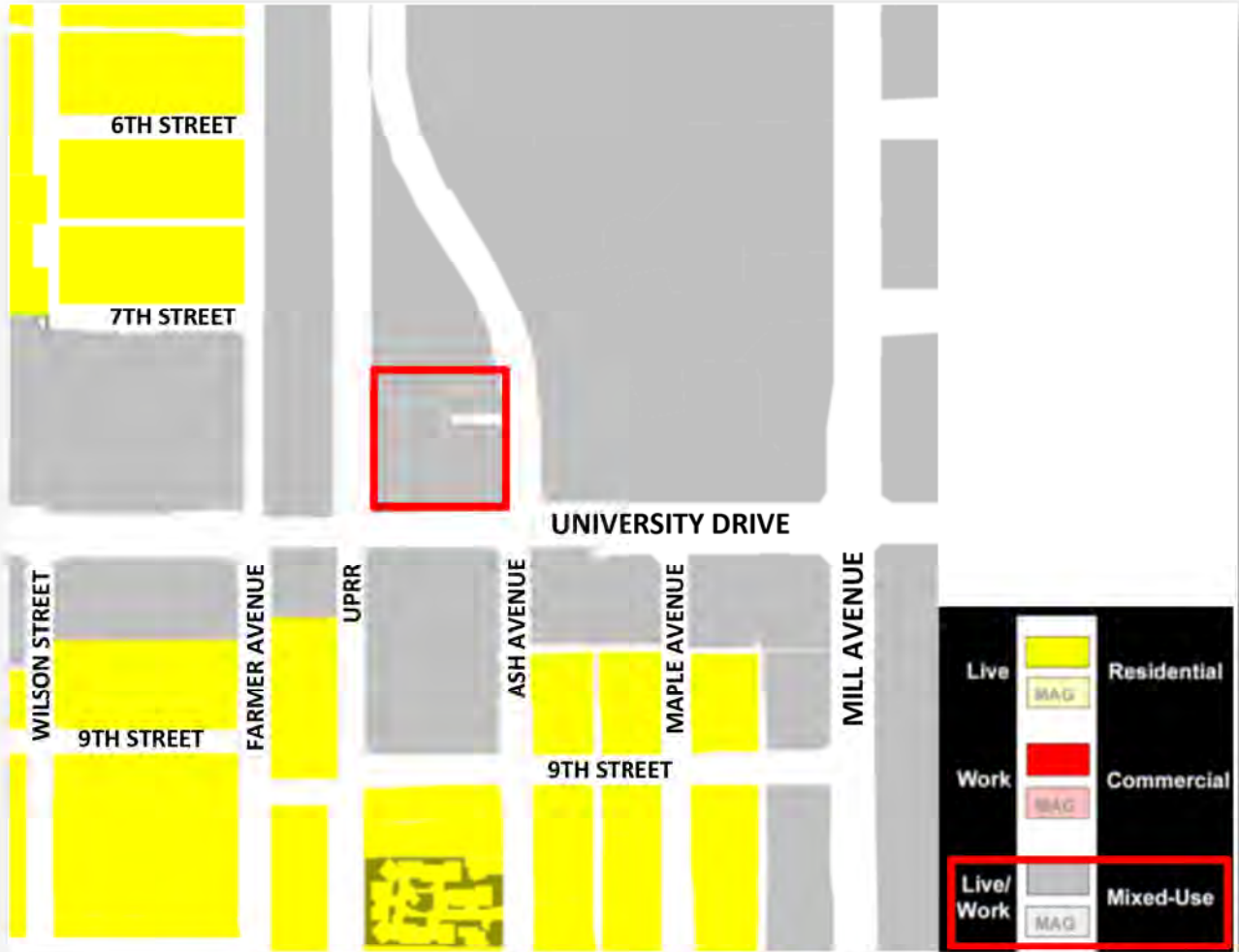


EXHIBIT - GENERAL PLAN 2040
PROJECTED RESIDENTIAL DENSITY MAP

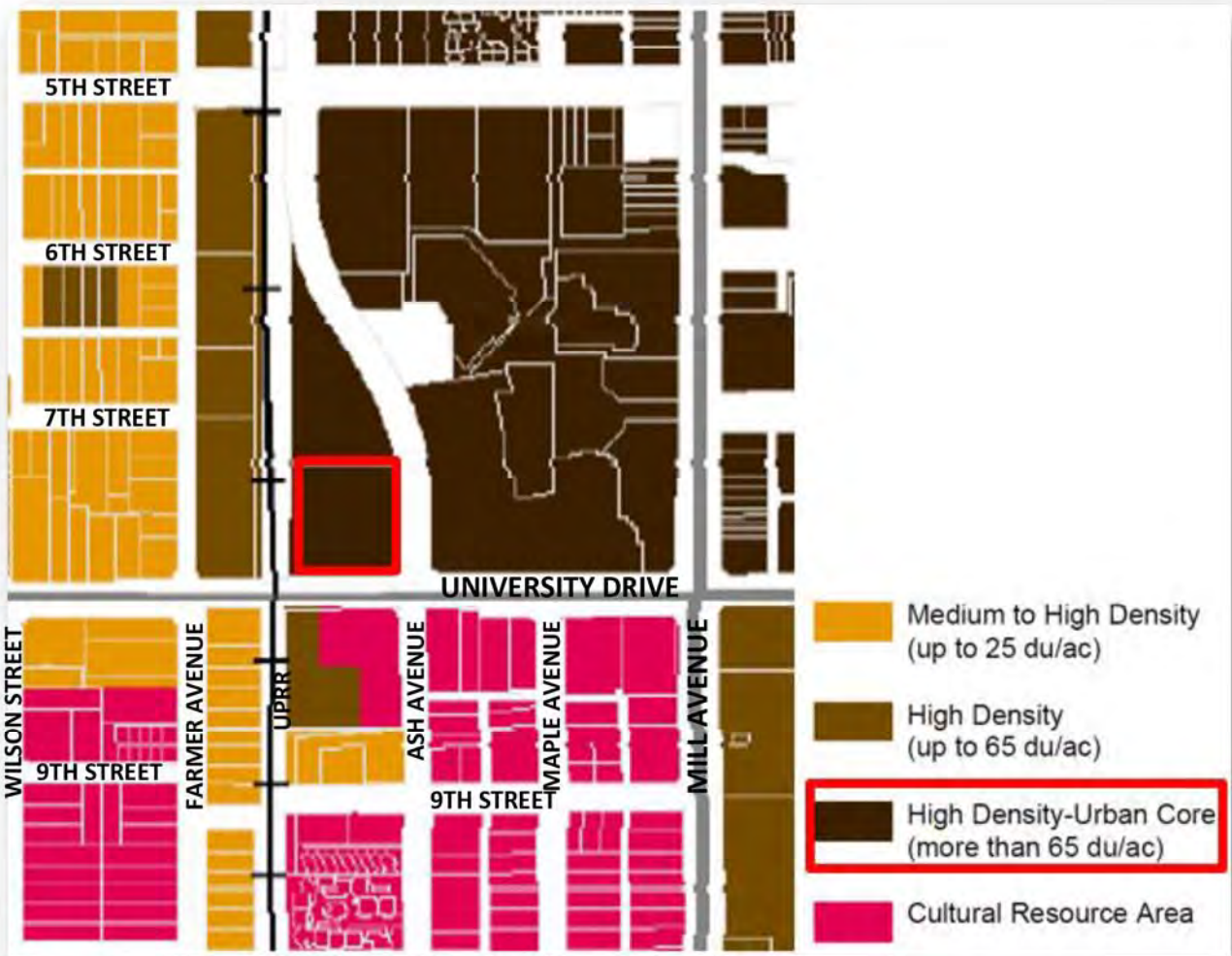
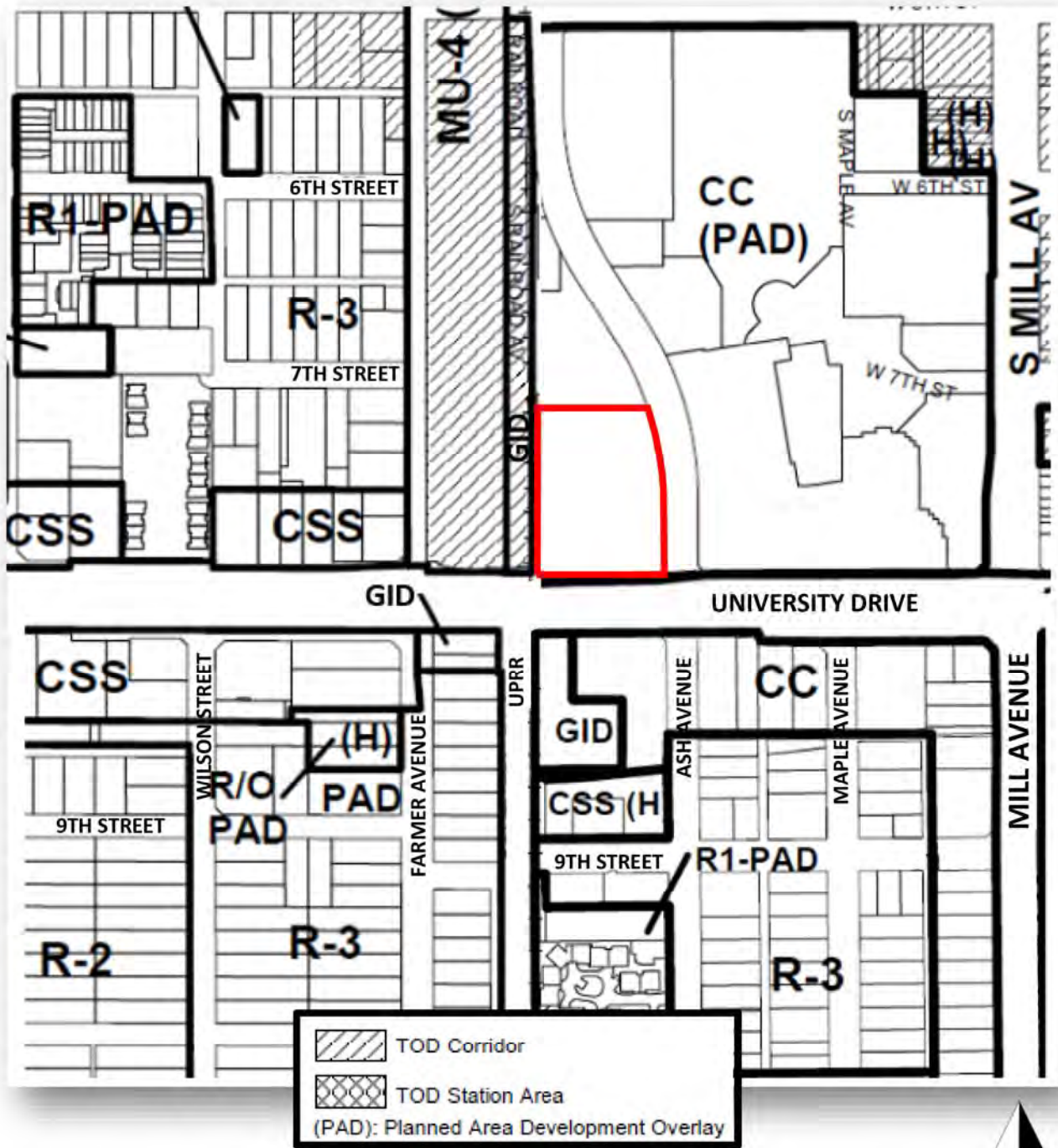


EXHIBIT - ZONING MAP



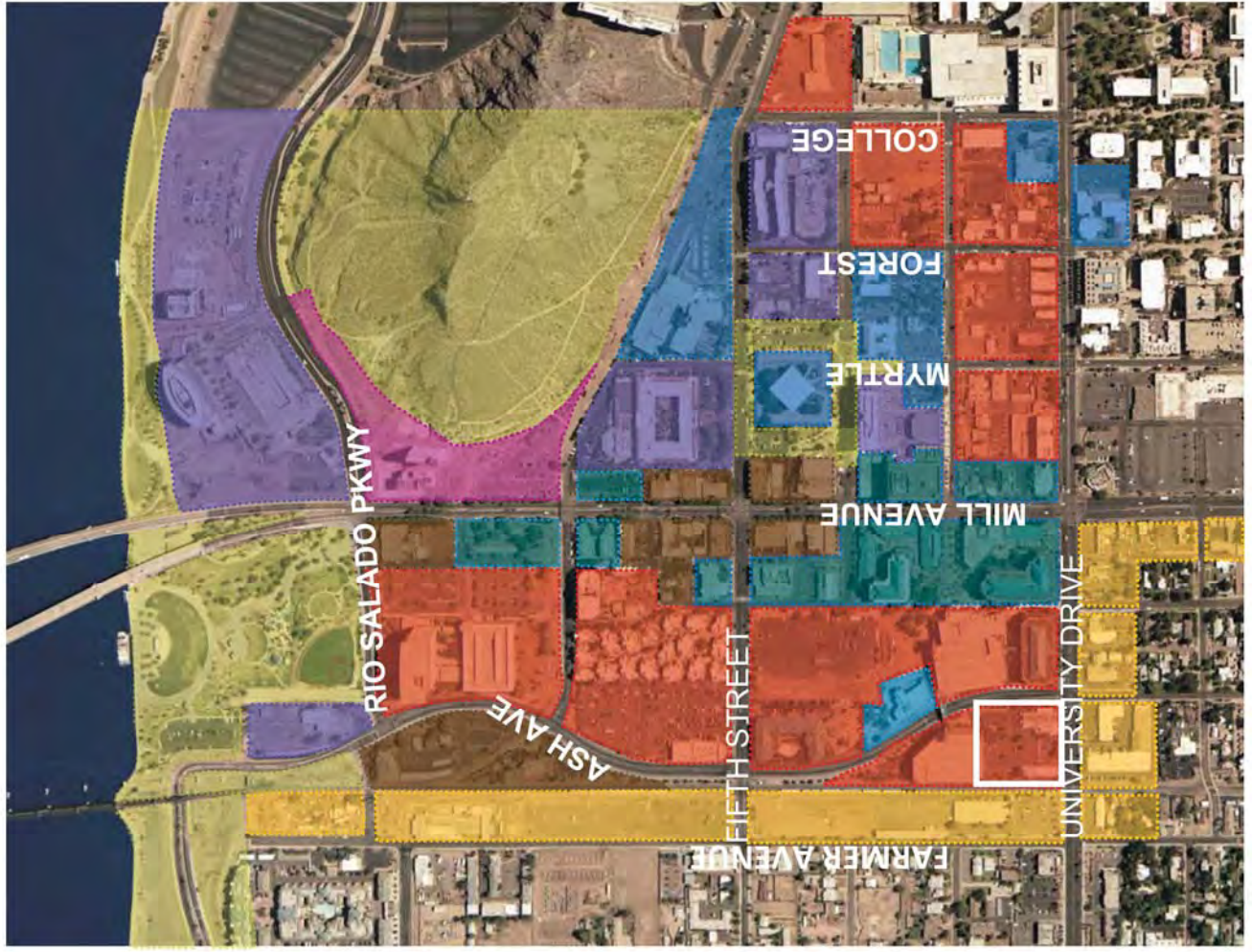
**EXHIBIT - COMMUNITY DESIGN PRINCIPLES
DOWNTOWN MILL AVENUE DISTRICT HEIGHT STUDY**

area	symbol	height
NEIGHBORHOOD BUFFER		50' max.
HERITAGE CORE		75' max. (15' stpbk. @50' height)
CIVIC/CULTURAL		75' max.
MILL AVENUE CORRIDOR		150' max. (15' stpbk. @50' height; 10' stpbk. @75' height)
URBAN- PARK TRANSITION		200' max. (20' stpbk. @75' height)
URBAN CENTER		300' max.
HAYDEN FERRY SOUTH SPECIAL STUDY AREA		See specific guidelines*
PARK/PRESERVE		

* "Hayden Ferry South Development Guidelines" were accepted by the Rio Salado Advisory Commission in March 2000, accepted by the Tempe City Council in August 2000 and amended by development agreement in April 2001.

Accepted 6 April 2006 (Central City Development Committee of the Whole)

**CONCEPT STUDY
DOWNTOWN BUILDING HEIGHTS**

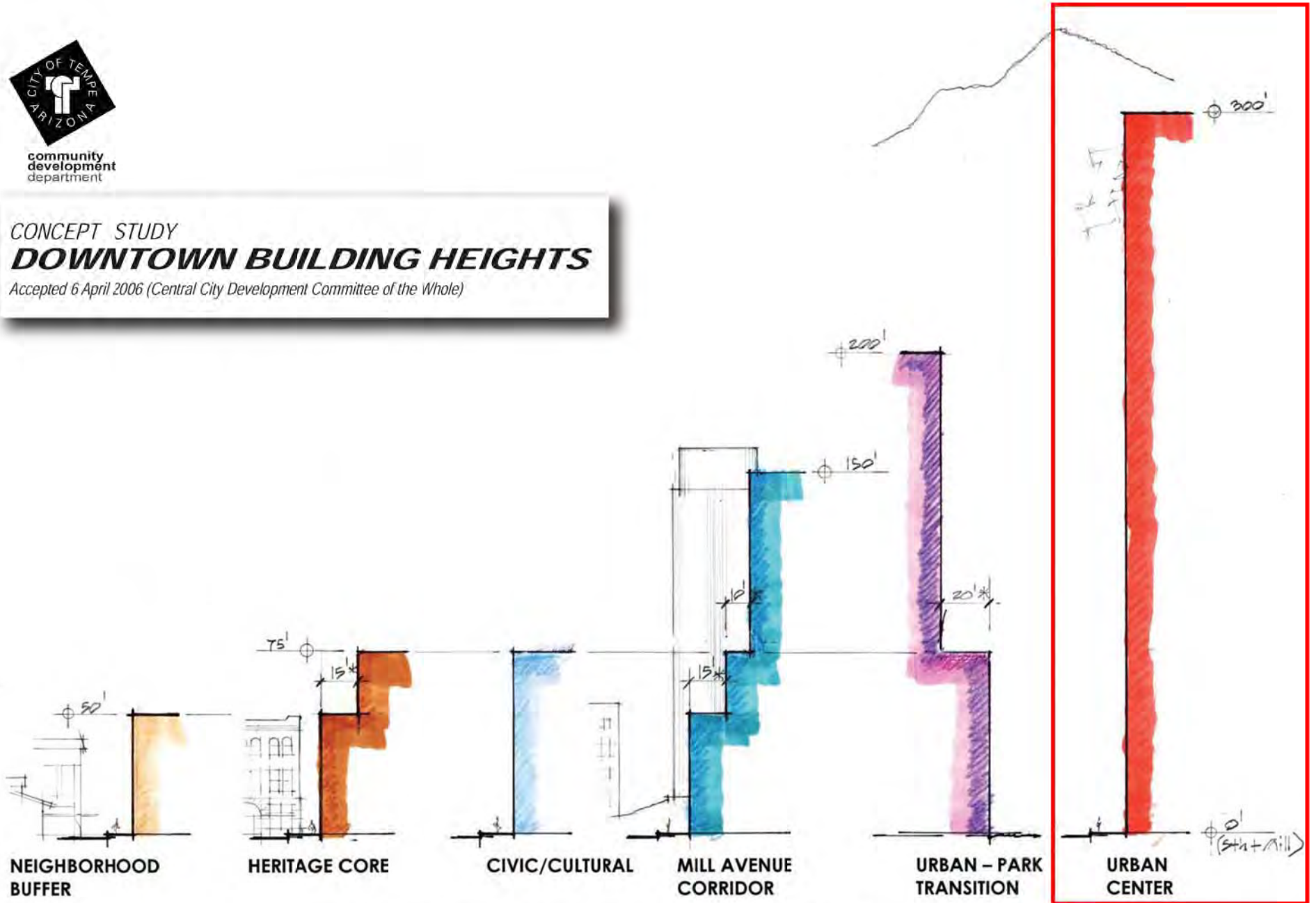




community development department

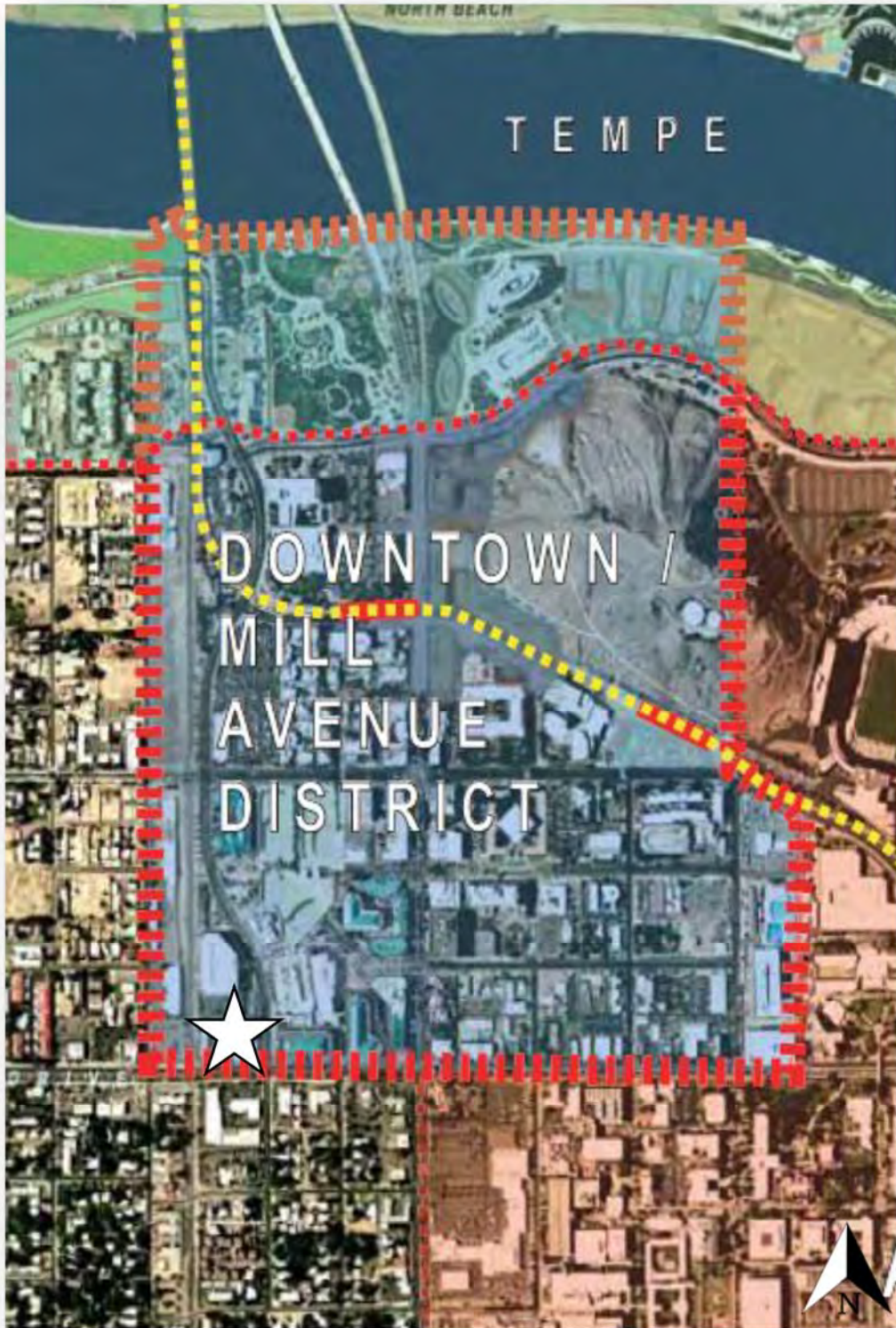
CONCEPT STUDY DOWNTOWN BUILDING HEIGHTS

Accepted 6 April 2006 (Central City Development Committee of the Whole)



* STEPBACKS are calculated as an average from street and/or park frontages, and pertain to primary building mass (additional articulation, utilizing balconies, canopies, etc., is not only encouraged, but expected).

EXHIBIT - DOWNTOWN/MILL AVENUE DISTRICT



**EXHIBIT - MILL & LAKE DISTRICT
STREETScape PRINCIPLES AND GUIDELINES**



PLANNED AREA DEVELOPMENT OVERLAY THE FOUNDRY

A PORTION OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 1 NORTH, RANGE 4 EAST OF THE GILA AND SALT RIVER AND MERIDIAN,
MARICOPA COUNTY, ARIZONA

ACKNOWLEDGMENT

ON THIS DAY OF _____ 20____ BEFORE ME, THE UNDERSIGNED, PERSONALLY APPEARED DAVID CLOCK, WHO ACKNOWLEDGED HIMSELF TO BE THE PERSON WHOSE NAME IS SUBSCRIBED TO THE INSTRUMENT WITHIN, AND WHO EXECUTED THE FOREGOING INSTRUMENT FOR THE PURPOSES THEREIN CONTAINED.

IN WITNESS WHEREOF, I HERETO SET MY HAND AND OFFICIAL SEAL

BY: _____ NOTARY PUBLIC _____ MY COMMISSION EXPIRES _____

FORUM REAL ESTATE GROUP, A COLORADO LIMITED LIABILITY COMPANY

BY: _____ OWNER _____ DATE _____

ITS DAVID CLOCK, MANAGER - DEVELOPER

LEGAL DESCRIPTION

PARCEL NO. 1:
LOT 1, COSMOPOLITAN, ACCORDING TO BOOK OF MAPS, PAGE 35, AND AFFIDAVIT OF CORRECTION IN RECORDING NO. 2007-0429022, RECORDS OF MARICOPA COUNTY, ARIZONA;

EXCEPT ANY MINE FOR GOLD, SILVER, CINNABAR OR COPPER, OR ANY VALID MINING CLAIM OR POSSESSION HELD UNDER THE EXISTING LAWS AS RESERVED IN PATENT RECORDED IN BOOK 15 OF DEEDS, PAGE 634.

PARCEL NO. 2:
AN EASEMENT FOR PEDESTRIAN BRIDGE EASEMENT RECORDED JULY 25, 1991 IN RECORDING NO. 91-345266 AND AMENDMENT TO PEDESTRIAN BRIDGE EASEMENT RECORDED AUGUST 27, 1993 IN RECORDING NO. 93-0575168 OF OFFICIAL RECORDS.

APPROVAL

APPROVED BY THE MAYOR AND CITY COUNCIL OF THE CITY TEMPE ON THIS ____ DAY OF _____, 2016

OWNER/DEVELOPER

DAVID CLOCK
FORUM REAL ESTATE GROUP
4500 CHERRY CREEK DRIVE SOUTH, SUITE 550
GLENDALE, COLORADO 80246
303.601.6871, DCLOCK@FORUMRE.COM

PROJECT DATA

COMMUNITY PLANNING OVERLAY	PAD PROPOSED (CITY)
GENERAL PLAN LAND USE	MIXED-USE
GENERAL PLAN DENSITY	HIGH DENSITY URBAN CORE (H45-DH4)
ZONING	CC PAD
SITE AREA	80,938.00 SF (1.858 AC)
DWELLING QUANTITY	292 UNITS (MIN 266, MAX 300 UNITS)
DENSITY	357.36 DUA (MIN 32.94, MAX 161.46 DUA)
BUILDING HEIGHT	117'-0" (MAX 130'-0" (TO TOP OF MECH))
BUILDING LOT COVERAGE	92.12% (74,540.00 SF/80,938.00 SF)
SITE LANDSCAPE COVERAGE	7.00% (1,819.00 SF/80,938.00 SF)
BUILDING SETBACKS:	
FRONT (UNIVERSITY)	0'-0" MAX
REAR (UNIVERSITY)	0'-0" MAX
SIDE (WEST)	0'-0" MAX
STREET SIDE (ASH)	0'-0" MAX
VEHICLE PARKING QUANTITY	557 SPACES
GROCCER PARKING	192 SPACES
TANDEM	0 TANDEM SPACES
RESIDENTIAL PARKING	365 SPACES
TANDEM	39 TANDEM SPACES
BICYCLE PARKING QUANTITY	312 SPACES
USES & AREAS	
TOTAL GROSS BUILDING AREA	572,278 SF
GROCCER (INCL. MEZZ.)	41,477 SF
LOBBY/LEASING/AMENITIES	10,586 SF
CLUB-HOUSE	6,837 SF
LOADING & TRASH	368 SF
MEP/STORAGE	9,014 SF
MAJOR VERT. PENETRATIONS	9,999 SF
PARKING	2,245,711 SF
GROSS RESIDENTIAL AREA	314,286 SF
NET RESIDENTIAL AREA	207,988 SF
USE PERMITS	
TANDEM PARKING	19 TANDEM STALLS

SITE VICINITY MAP



CONDITIONS OF APPROVAL: PADXXXXX

NO.	DATE	REVISION

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REC16058

THE FOUNDRY
284 W. UNIVERSITY DR
TEMPE, ARIZONA 85281
FORUM REAL ESTATE GROUP

PL160099

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DS160108

CITY PLANNING SUBMITTAL - 05.05.2016
SHEET COVER

SHEET NO.

A101

HPA 15257

DS160108

PL160099

REC16058

PLANNED AREA DEVELOPMENT OVERLAY THE FOUNDRY SITE PLAN

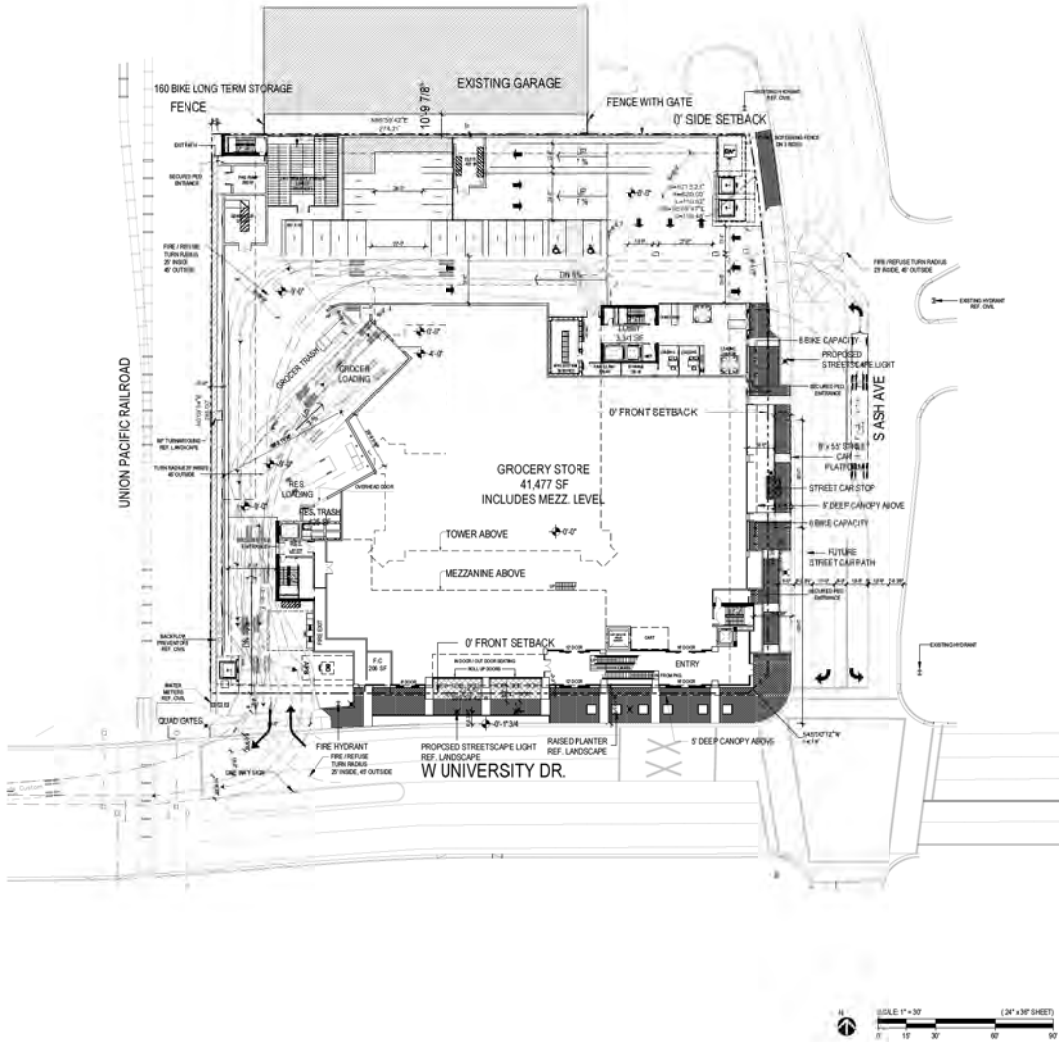


TABLE 4-20(1)	CC DEV. STANDARDS	SFD 2006.11 (DOWD AMENDMENTS)	PAD PROVIDED (CC PAD)
GENERAL PLAN LAND USE	MIXED-USE	MIXED-USE	MIXED-USE
GENERAL PLAN DENSITY	HIGH DENSITY (≥25 DU/A)	HIGH DENSITY (≥25 DU/A)	HIGH DENSITY URBAN CORE (4-65 DU/A)
SITE AREA	-N/A-	84,070.84 SF (1.93 AC)	80,938.91 SF (1.85 AC)
DWELLING QUANTITY	-N/A-	187 UNITS	793 UNITS (MIN 245, MAX 300 UNITS)
DENSITY	-N/A-	16.90 DU/A	137.18 DU/A (MIN 133.84, MAX 161.48 DU/A)
BUILDING HEIGHT	50-40'	247'-0"	137'-4" (MAX 130'-0" TO TOP OF MCH)
BUILDING LOT COVERAGE	-N/A-	70% (84,070.84 SF/84,070.84 SF)	70.12% (74,560.00 SF/80,938.00 SF)
SITE LANDSCAPE COVERAGE	-N/A-	1.2% (1,009.85 SF/84,070.84 SF)	2.00% (1,618.00 SF/80,938.00 SF)
MUNICIPAL SETBACKS			
FRONT (UNIVERSITY)	20'-0"	5'-0"	0'-0" MAX
REAR (NORTH)	0'-0"	7'-0"	0'-0" MAX
SIDE (WEST)	0'-0"	20'-0"	0'-0" MAX
STREET SIDE (ASH)	0'-0"	5'-0"	0'-0" MAX
REQUIRED VEHICLE PARKING	(CC STANDARDS ORD. 02015.40)	(TABLE 4-60(1)(E))	(CC STANDARDS ORD. 02015.40)
GROCER	7.91 (41,477 / 5,200 / 500)	177 (8,377 SF / 300)	72.91 (11,477 / 5,200 / 500)
ACCESSORY RESTAURANT	4.00 (1,200 / 300 / 500)	0.00	0.00 (1,200 / 300 / 500)
ACCESSORY OUTDOOR PATIO	4.00 (600 / 150)	0.00	0.00 (600 / 150)
1 BEDROOM RESIDENTIAL	33.06 (268.88 * 0.5/80)	178 (113 * 1.5/UNIT)	133 (202/6.88 * 0.5/80)
2 BEDROOM RESIDENTIAL	11.00 (43.88 * 0.5/80)	132 (66 * 2.0/UNIT)	71.00 (42.88 * 0.5/80)
3 BEDROOM RESIDENTIAL	4.50 (15.88 * 0.5/80)	45 (18 * 2.5/UNIT)	4.50 (15.88 * 0.5/80)
GUEST RESIDENTIAL	9.20 (282 * 0.5/UNIT)	37 (187 * 0.5/UNIT)	37 (20 * 0.5/UNIT)
PARKING PROVIDED	311 SPACES	541 SPACES	261 SPACES
PROVIDED VEHICLE PARKING			
GROCER PARKING			187 SPACES
STANDARD			180 SPACES
TANDEM			0 TANDEM SPACES
COMPACT			28 SPACES
ADA			4 SPACES
RESIDENTIAL PARKING			
STANDARD			276 SPACES
TANDEM			19 TANDEM SPACES
COMPACT			45 SPACES
ADA			5 SPACES
BIKECYCLE PARKING QUANTITY	(BIKECYCLE COMMUNITY AREA)	(BIKECYCLE COMMUNITY AREA)	(BIKECYCLE COMMUNITY AREA)
GROCER	1.53 (41,477 / 7,300) (MIN 4)	13 (51,171 SF / 7,300 SF)*	1.53 (41,477 / 7,300) (MIN 4)
ACCESSORY RESTAURANT	1.40 (1,200 / 500)	0.00	1.40 (1,200 / 500)
ACCESSORY OUTDOOR PATIO	6.65 (600 / 7,300)	0.00	0.45 (190 / 7,300)
1 BEDROOM RESIDENTIAL	89.50 (268.88 * 0.75/UNIT)	85 (113 * 0.75/UNIT)	109 (60/6.88 * 0.75/UNIT)
2 BEDROOM RESIDENTIAL	5.75 (21 * 0.75/UNIT)	42 (18 * 0.75/UNIT)	15.75 (11 * 0.75/UNIT)
3 BEDROOM RESIDENTIAL	1.00 (5 * 1.0/UNIT)	18 (18 * 1.0/UNIT)	5.00 (5 * 1.0/UNIT)
GUEST RESIDENTIAL	18.40 (282 * 0.5/UNIT)	37 (187 * 0.5/UNIT)	18.40 (192 * 0.5/UNIT)
PARKING REQUIRED	311.03 SPACES	155.50 SPACES	261.00 SPACES
PARKING PROVIDED		292 SPACES	312 SPACES
USES & AREAS			
TOTAL GROSS BUILDING AREA		733,439 SF	572,278 SF
GROCER (INCL. MEZZ.)		53,172 SF	41,477 SF
LOBBY/LEASING/AMENITIES		-N/A-	10,586 SF
CLUB HOUSE		-N/A-	6,837 SF
LOADING & TRASH		-N/A-	148 SF
MEP/STORAGE		-N/A-	9,014 SF
MAJOR VERT. PENETRATIONS		-N/A-	9,893 SF
PARKING		-N/A-	289,711 SF
GENERAL COMMERCIAL		53,172 SF	0.00 SF
GROSS RESIDENTIAL AREA		380,453 SF	344,341 SF
NET RESIDENTIAL AREA		-N/A-	292,868 SF
USE PERMITS			
TANDEM PARKING		-N/A-	19 TANDEM SPACES

MVP (major vertical penetrations) on or overtopping a floor to accommodate vertical building elements such as stairs, elevators and HVAC shafts.
Net residential area: the source linkage from exterior face of all exterior frame walls that enclose air space (excluding side of corridor walls) within the footprint of the building.
Gross residential area: the floor area including MEP, MEP/storage, retail and parking.
Total gross building area: the floor area within the perimeter of the exterior walls.

REVISION	DATE	COMMENTS

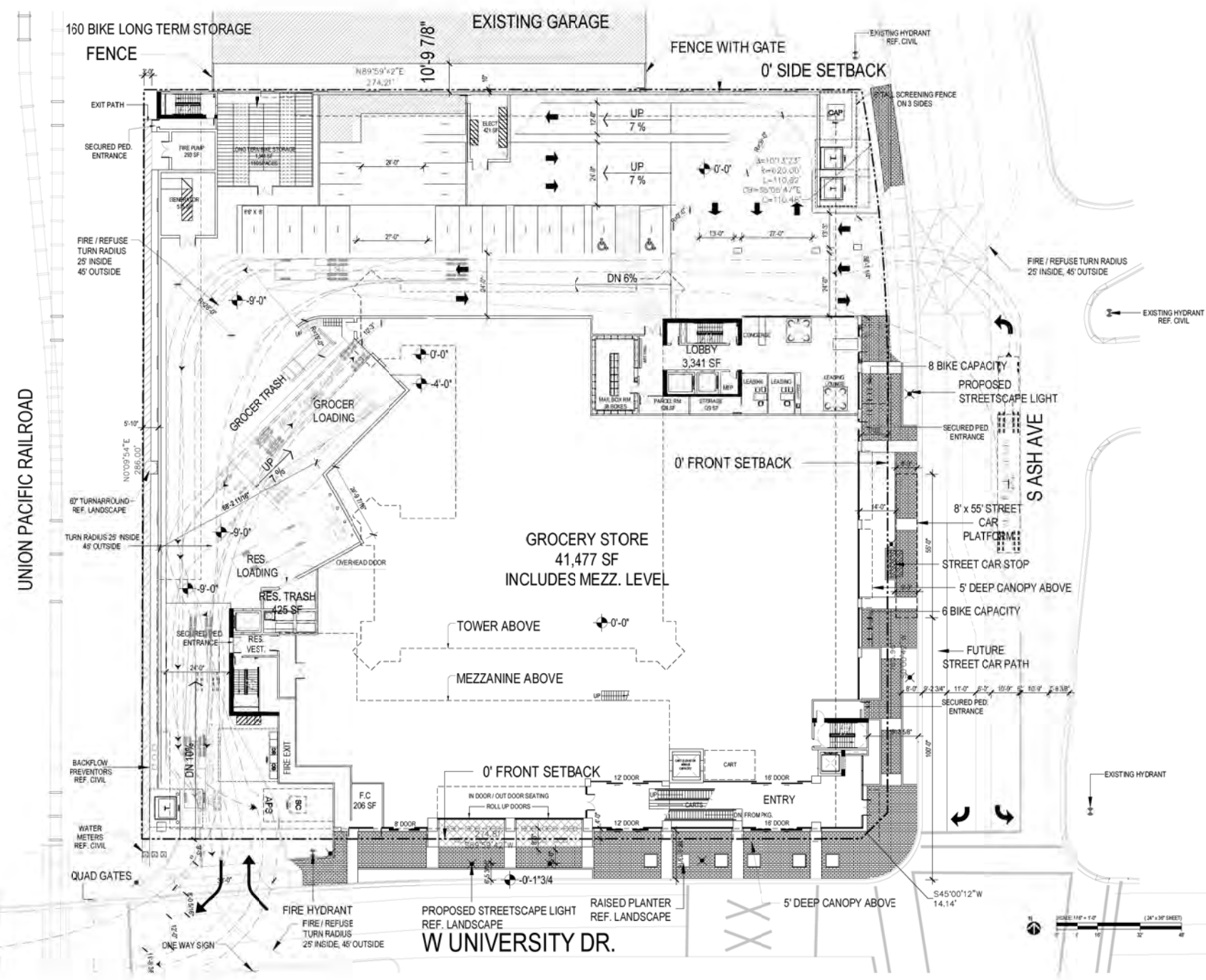
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THE FOUNDRY
 284 W UNIVERSITY DR
 TEMPE, ARIZONA 85281
 FORUM REAL ESTATE GROUP

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CITY PLANNING SUBMITTAL - 05.05.2016
 SHEET NO. 68027-00
 A102
 HPA 150267



REVISIONS

#	DATE	COMMENTS

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DRAWN BY: J. G. / CHECKED BY: J. G. / DATE: 05/05/2016

SHEET 1 OF 12
SITE PLAN

A103

HPA 15-057

PROJECT DATA

TABLE 4-203(B)	CC DEV. STANDARDS	SPD-2006.53 (COSMO AMENDED)	PAD PROVIDED (CC PAD)
GENERAL PLAN LAND USE	MIXED-USE	MIXED-USE	MIXED-USE
GENERAL PLAN DENSITY	HIGH DENSITY (>25 DUA)	HIGH DENSITY (>25 DUA)	HIGH DENSITY URBAN CORE (>65 DUA)
SITE AREA	-N/S-	84,070.84 SF (1.93 AC)^	80,938.00 SF (1.858 AC)
DWELLING QUANTITY	-N/S-	187 UNITS	292 UNITS (MIN 260, MAX 300 UNITS)
DENSITY	-N/S-	96.90 DUA	157.16 DUA (MIN 139.94, MAX 161.46 DUA)
BUILDING HEIGHT	50'-0"	247'-0"	117'-4" (MAX 130'-0") (TO TOP OF MECH)
BUILDING LOT COVERAGE	-N/S-	70% (58,849.56 SF/84,070.80 SF)	92.12% (74,560.00 SF/80,938.00 SF)
SITE LANDSCAPE COVERAGE	-N/S-	1.2% (1,008.85 SF/84,070.80 SF)	2.00% (1,619.00 SF/80,938.00 SF)
BUILDING SETBACKS			
FRONT (UNIVERSITY)	0'-0"	5'-0"	0'-0" MAX
REAR (NORTH)	0'-0"	2'-0"	0'-0" MAX
SIDE (WEST)	0'-0"	20'-0"	0'-0" MAX
STREET SIDE (ASH)	0'-0"	5'-0"	0'-0" MAX
REQUIRED VEHICLE PARKING	(CC STANDARDS ORD. O2015.60)	(TABLE 4-603(E))	(CC STANDARDS ORD. O2015.60)
GROCER	72.95 (41,477-5,000 / 500)	177 (53,172 SF / 300)	72.95 (41,477-5,000 / 500)
ACCESSORY RESTAURANT	0.00 (1,200-5,000 / 500)	0.00	0.00 (1,200-5,000 / 500)
ACCESSORY OUTDOOR PATIO	0.00 (900 / 0.00)	0.00	0.00 (900 / 0.00)
1-BEDROOM RESIDENTIAL	133.00 (266 BR * 0.5/BR)	170 (113 * 1.5/UNIT)	133.00 (266 BR * 0.5/BR)
2-BEDROOM RESIDENTIAL	21.00 (42 BR * 0.5/BR)	112 (56 * 2.0/UNIT)	21.00 (42 BR * 0.5/BR)
3-BEDROOM RESIDENTIAL	4.50 (15 BR * 0.3/BR)	45 (18 * 2.5/UNIT)	4.50 (15 BR * 0.3/BR)
GUEST RESIDENTIAL	29.20 (292 * 0.1/UNIT)	37 (187 * 0.2/UNIT)	29.20 (292 * 0.1/UNIT)
PARKING REQUIRED:	261 SPACES	541 SPACES	261 SPACES
PROVIDED VEHICLE PARKING		646 SPACES	557 SPACES
GROCER PARKING			192 SPACES
STANDARD			160 SPACES
TANDEM			0 TANDEM SPACES
COMPACT			28 SPACES
ADA			4 SPACES
RESIDENTIAL PARKING			365 SPACES
STANDARD			276 SPACES
TANDEM			19 TANDEM SPACES
COMPACT			45 SPACES
ADA			6 SPACES
BICYCLE PARKING QUANTITY	(BICYCLE COMMUTE AREA)	(BICYCLE COMMUTE AREA)	(BICYCLE COMMUTE AREA)
GROCER	5.53 (41,477 / 7,500) (MIN 4)	13 (53,172 SF / 7,500 SF)*	5.53 (41,477 / 7,500) (MIN 4)
ACCESSORY RESTAURANT	2.40 (1,200 / 500)	0.00	2.40 (1,200 / 500)
ACCESSORY OUTDOOR PATIO	0.45 (900 / 2,000)	0.00	0.45 (900 / 2,000)
1-BEDROOM RESIDENTIAL	199.50 (266 * 0.75/UNIT)	85 (113 * 0.75/UNIT)	199.50 (266 * 0.75/UNIT)
2-BEDROOM RESIDENTIAL	15.75 (21 * 0.75/UNIT)	42 (56 * 0.75/UNIT)	15.75 (21 * 0.75/UNIT)
3-BEDROOM RESIDENTIAL	5.00 (5 * 1.0/UNIT)	18 (18 * 1.0/UNIT)	5.00 (5 * 1.0/UNIT)
GUEST RESIDENTIAL	58.40 (292 * 0.2/UNIT)	37 (187 * 0.2/UNIT)	58.40 (292 * 0.2/UNIT)
PARKING REQUIRED:	287.03 SPACES	195 SPACES	287.03 SPACES
PARKING PROVIDED:		262 SPACES	312 SPACES
USES & AREAS			
TOTAL GROSS BUILDING AREA		733,419 SF	572,278 SF

GROCER (INCL. MEZZ.)		53,172 SF	41,477 SF
LOBBY/LEASING/AMENITIES		-UNK-	10,586 SF
CLUBHOUSE		-UNK-	6,837 SF
LOADING & TRASH		-UNK-	368 SF
MEP/STORAGE		-UNK-	9,014 SF
MAJOR VERT. PENETRATIONS		-UNK-	9,999 SF
PARKING		-UNK-	249,711 SF
GENERAL COMMERCIAL		53,172 SF	0.00 SF
GROSS RESIDENTIAL AREA		385,653 SF	244,286 SF
NET RESIDENTIAL AREA		-UNK-	207,988 SF
USE PERMITS			
TANDEM PARKING		-N/A-	19 TANDEM STALLS

MVP (major vertical penetrations): major openings in a floor to accommodate vertical building elements such as stairs, elevators and HVAC shafts.

Net residential area: the square footage from exterior face of all exterior frame walls that enclose air space corridor side of corridor walls center line of demising walls). It does not include patios, balconies, Patio/balcony storage.

Gross residential area: the floor area excluding MVP, MEP/storage, retail and parking.

Total gross building area: the floor area within the perimeter of the exterior walls



MASSING

SIDE VIEW REVISED

#	DATE	COMMENTS

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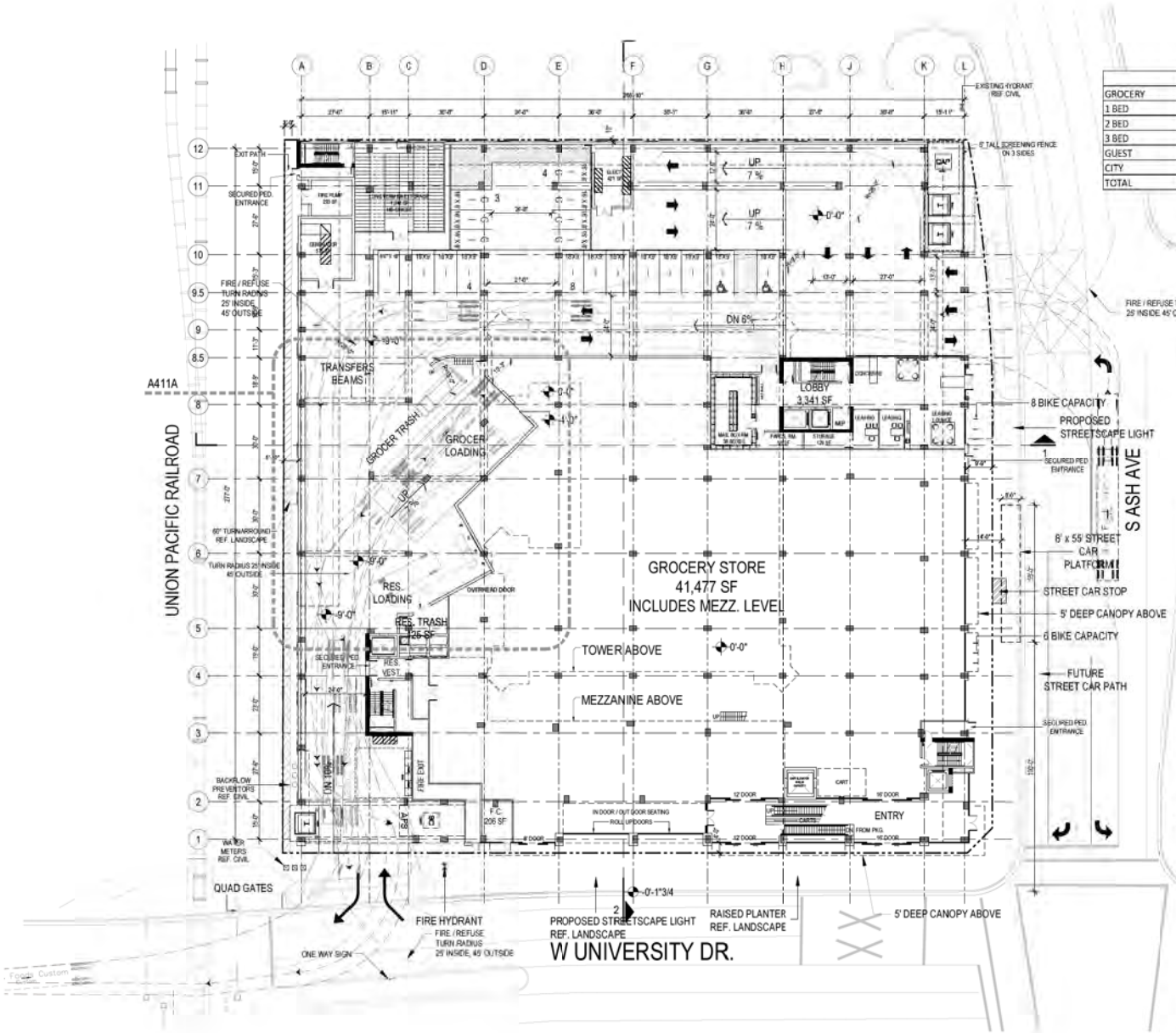
THE FOUNDRY
 234 W. UNIVERSITY DR.
 TEMPE, ARIZONA 85281
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SHEET COMPLETED: **MASSING**
 SHEET NO.: **A104**
 HPA 15267

CITY PLANNING SUBMITTAL: 05/05/2016



	STANDARD	COMPACT	ADA	TANDEM	TOTAL
GROCERY	-	-	-	-	-
1 BED	-	-	-	-	-
2 BED	-	-	-	-	-
3 BED	-	-	-	-	-
GUEST	6	1	2	-	9
CITY	4	6	-	-	10
TOTAL	10	7	2	-	19

	GROUND LEVEL
BUILDING AREA:	74,560 SF
MEP/STORAGE:	2,964 SF
TRASH/LOADING:	368 SF
LOBBY/LEASING:	3,036 SF
GROCERY STORE:	34,495 SF
SERVICES:	33,695 SF
SURFACE PARKING:	19 SPACES

REVISIONS		
#	DATE	COMMENTS

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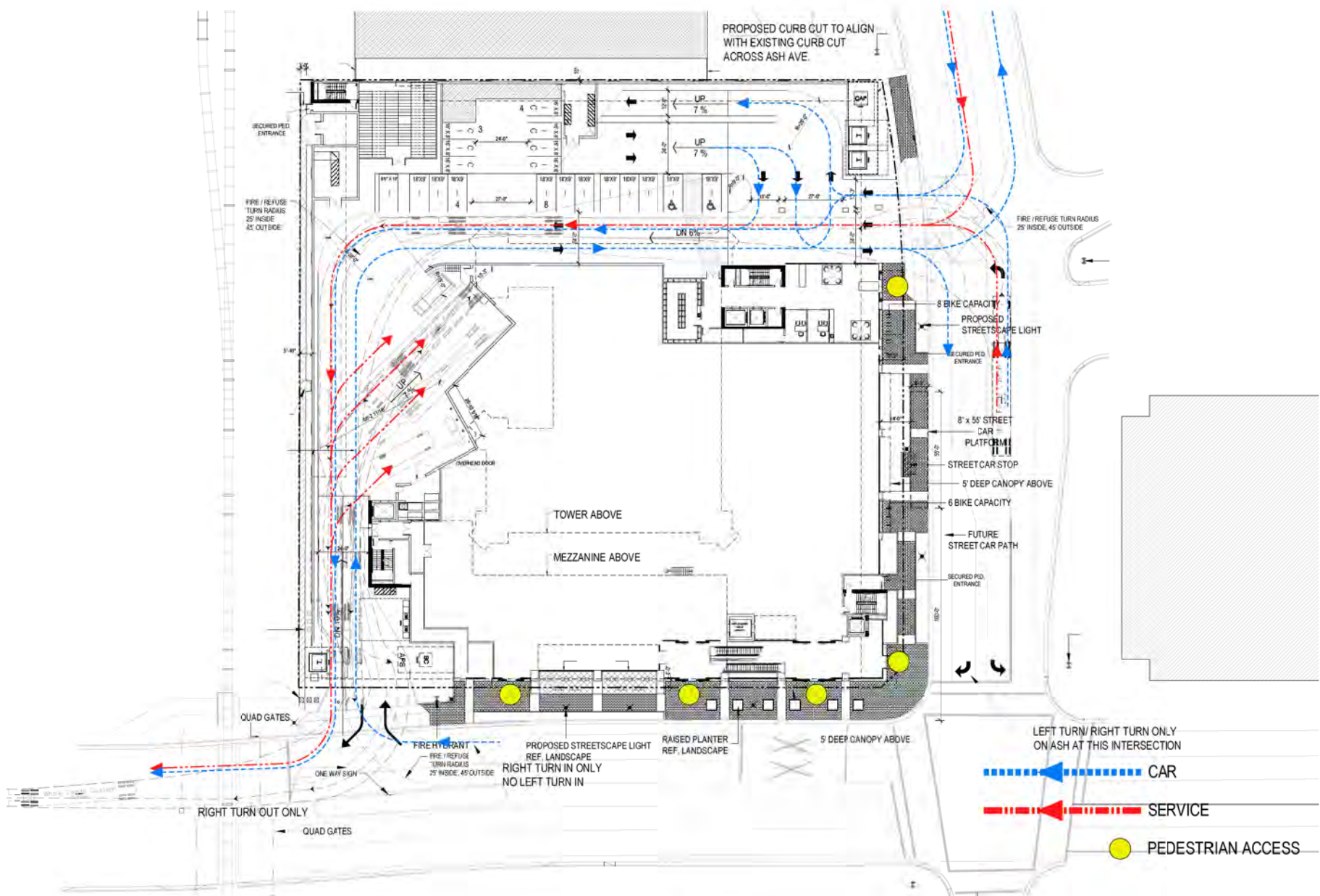
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 NEWPORT BEACH/SOFTBALL-FORUM/ST. LOUIS/ST. LOUIS/ST. LOUIS/ST. LOUIS/ST. LOUIS

SHEET OWNERS:
GROUND LEVEL FLOOR PLAN
 SHEET NO.

A411
 HP4 15.25P



TRAFFIC DIAGRAM

REVISIONS

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NEWPORT BEACH/SCOTTSDALE/TORONTO/CHENNAI/COUMBAHUR/DELHI/NEW DELHI

CITY PLANNING SUBMITTAL - 05.05.2016

SHEET COMMENTS:
TRAFFIC DIAGRAM

SHEET NO:
A110

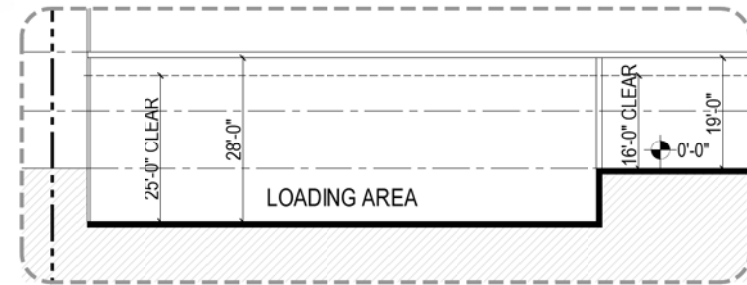
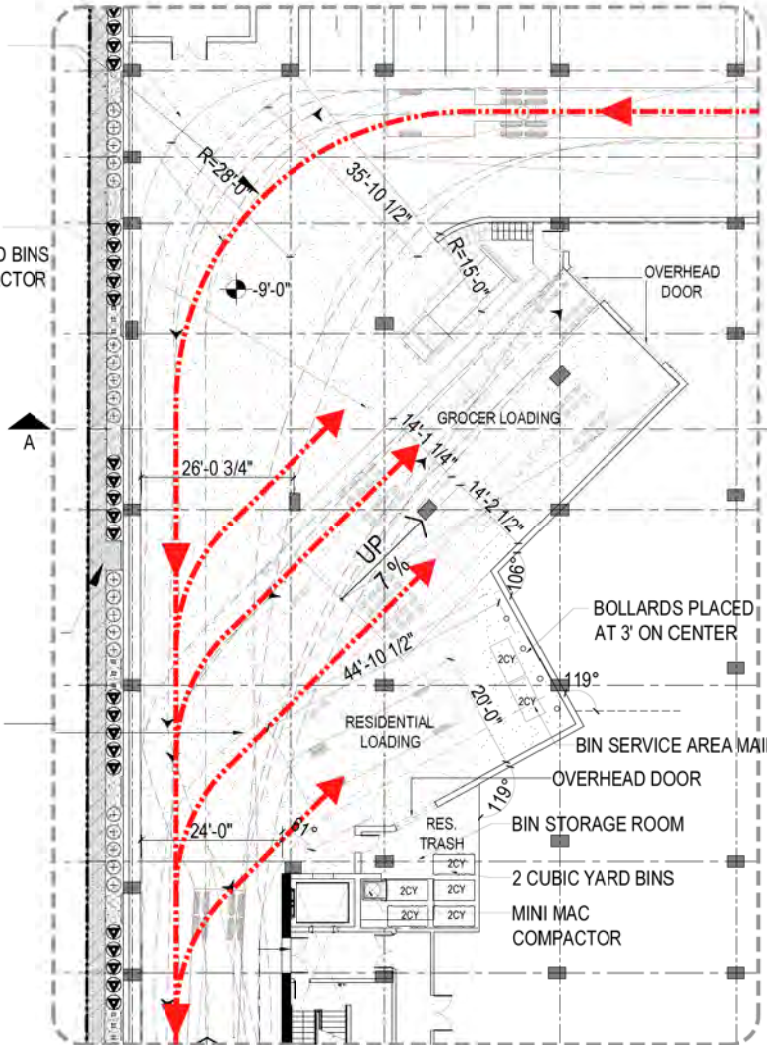
HPA 15.267

HATCHED AREA HAS A 25' CLEAR FLOOR HEIGHT FOR REFUSE SERVICING

TURN RADIUS
35' INSIDE
55' OUTSIDE

WHOLE FOOD HAS
REQUESTED 4 10YRD BINS
IN PLACE OF COMPACTOR

15' MINIMUM CLEAR HEAD HEIGHT
ALONG SERVICE DRIVE PATH



SECTION A

BIN SERVICE AREA MAINTENANCE TO MOVE BINS FOR PICK UP AS NEEDED

A MINIMUM OF 3 PICK UPS PER WEEK SHALL BE PROVIDED

Data Entry	Quantity Results		Uncompacted Trash Bins		Compacted	
	Uncompacted Trash (lbs. per day)	Uncompacted Cu. Yd. (per day)	Number of 4 cu. yd. bins (Assume 1 pickup / week)	Number of 6 cu. yd. bins (Assume 1 pickup / week)	Number of 2 cu. yd. bins (Assume 1 pickup / week)	Number of 2.75 cu. yd. bins (Assume 1 pickup / week)
Enter Number of Apartment Units	290					
Enter Estimated Trash per Household (lbs. per day) National avg. = 5.37 lbs. per apartment dweller.	1,557	14.99	18.1	12.1	14.5	10.5
How many pounds in a Cubic Yard? (200 lbs. default estimate)		Uncompacted Cu. Yd. (per week)				
Compression Ratio for Compactor. (0.40 is default for 40%)	1.4	10,495	72.4			

Enter number of units in the Yellow box. Enter compression ratio of desired Trash Compactor in Green box. White cells are based on National data from sources below.

Sources: www.epa.gov/osd, EPA-561-F-03-012, Appendix C - Waste Generation Calculations (Waste Prevention, Recycling and Composting Options) Version from RWIS 5, Commission.

Notes: All Public Works Department bins "pounds per cubic yard" are equivalent (48 lbs per cubic yard, 11.8 cubic yards per 4 cu. yd. bin, 2.75 cu. yd. bin)

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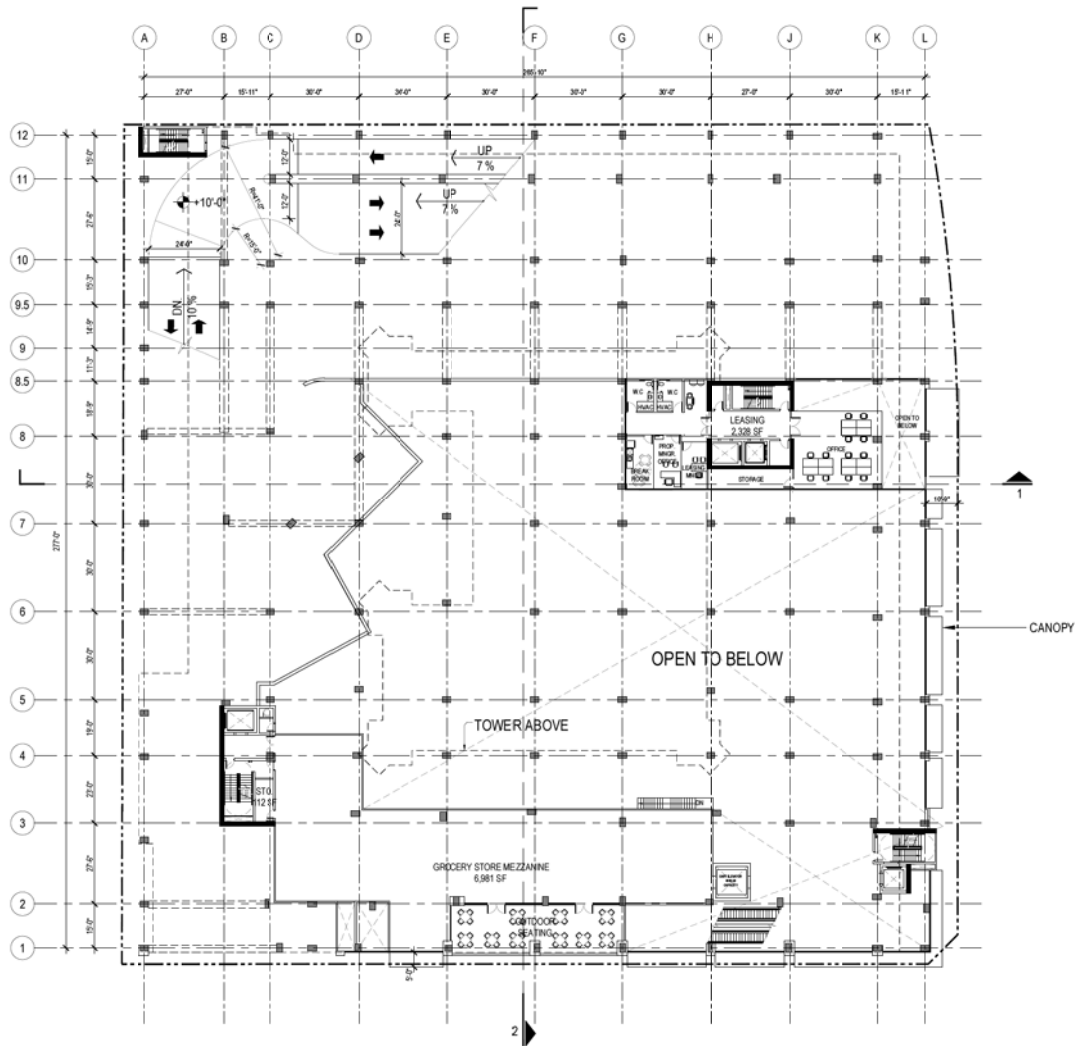
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ENLARGED LOADING AREA



A411a
HPA 15.257



STORE MEZZANINE LEVEL	
BUILDING AREA:	11,326 SF
LOBBY / LEASING:	2,328 SF
GROCERY STORE:	6,981 SF
SERVICES:	2,017 SF



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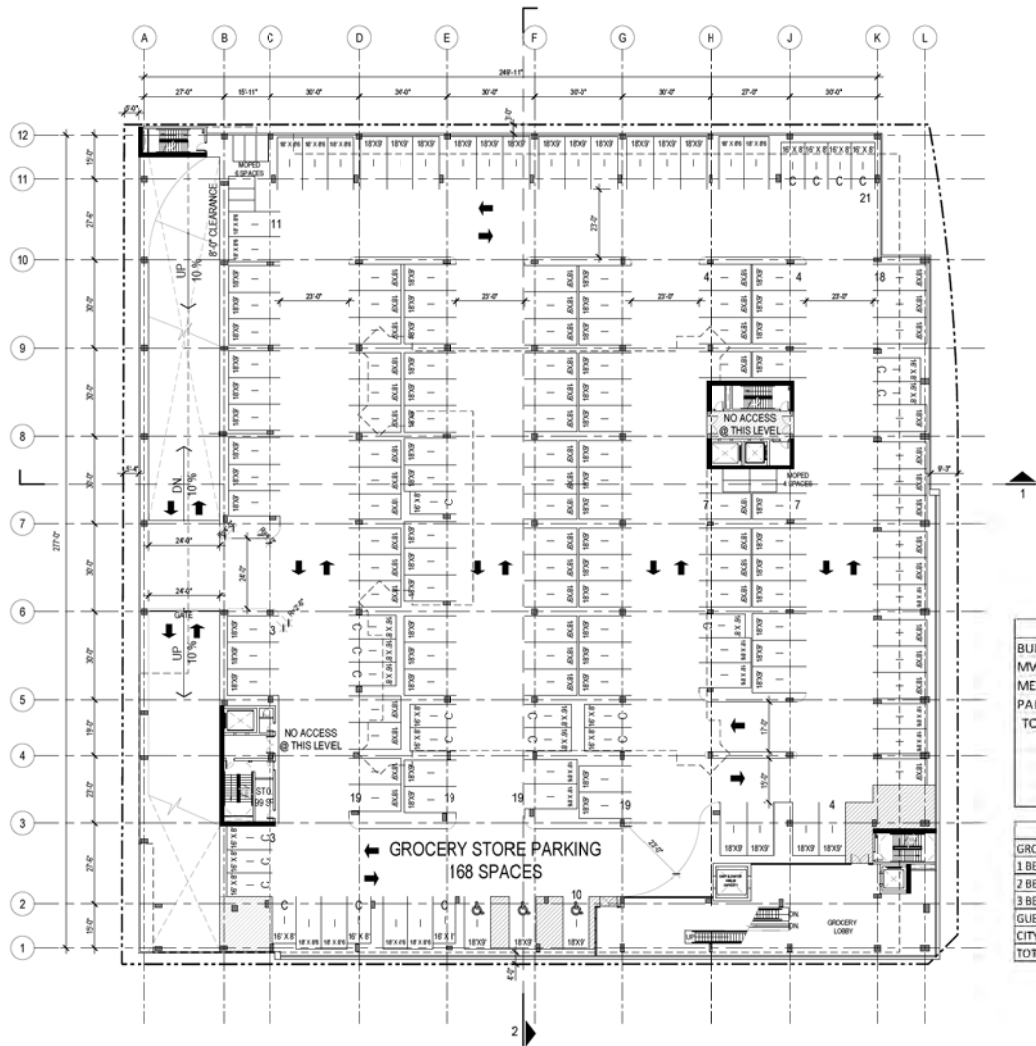
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PROJECT: MEZZANINE LEVEL

SHEET NO.

A412

HP4 15.257



LEVEL 2	
BUILDING AREA:	75,703 SF
MVP:	1,820 SF
MEP/STORAGE:	112 SF
PARKING/SERVICES:	73,771 SF
TOTAL GROCERY PARKING	168 SPACES
STANDARD	142 SPACES
COMPACT	23 SPACES
ADA	3 SPACES
TANDEM	- SPACES

	STANDARD	COMPACT	ADA	TANDEM	TOTAL
GROCERY	142	23	3	-	168
1 BED	-	-	-	-	-
2 BED	-	-	-	-	-
3 BED	-	-	-	-	-
GUEST	-	-	-	-	-
CITY	-	-	-	-	-
TOTAL	142	23	3	-	168

*min required by city is only 8'6" x 18'



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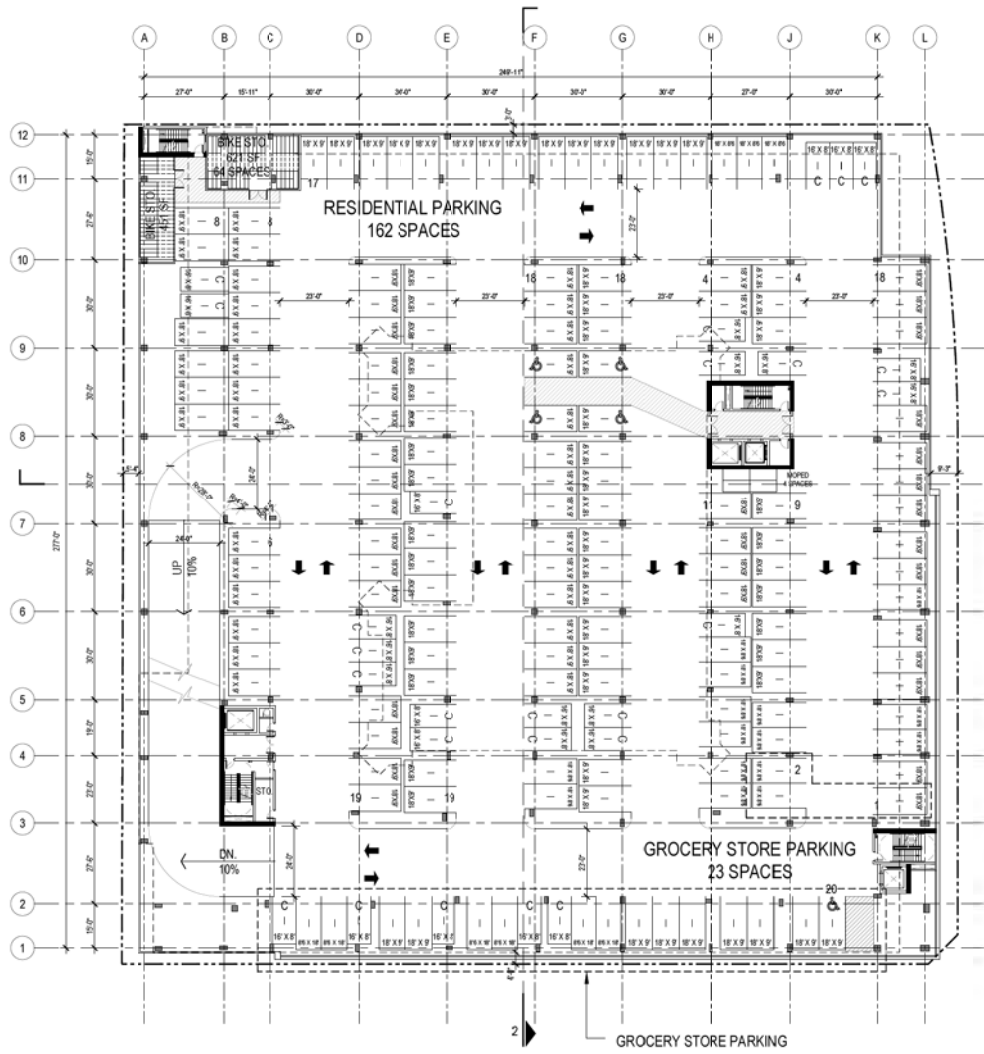
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SHEET CONTAINS:
LEVEL 2 FLOOR PLAN

A413

HP4 15.257



LEVEL 3	
BUILDING AREA:	74,703 SF
MVP:	1,097 SF
MEP/STORAGE:	1,265 SF
PARKING/SERVICES:	72,341 SF
TOTAL RES. PARKING	162 SPACES
STANDARD	129 SPACES
COMPACT	21 SPACES
ADA	4 SPACES
TANDEM	8 SPACES
TOTAL GROCERY PARKING	23 SPACES
STANDARD	17 SPACES
COMPACT	5 SPACES
ADA	1 SPACES

	STANDARD	COMPACT	ADA	TANDEM	TOTAL
GROCERY	17	5	1	-	23
1 BED	91	20	1	-	112
2 BED	16	-	1	5	22
3 BED	3	-	1	3	7
GUEST	19	1	1	-	21
CITY	-	-	-	-	-
TOTAL	146	26	5	8	185

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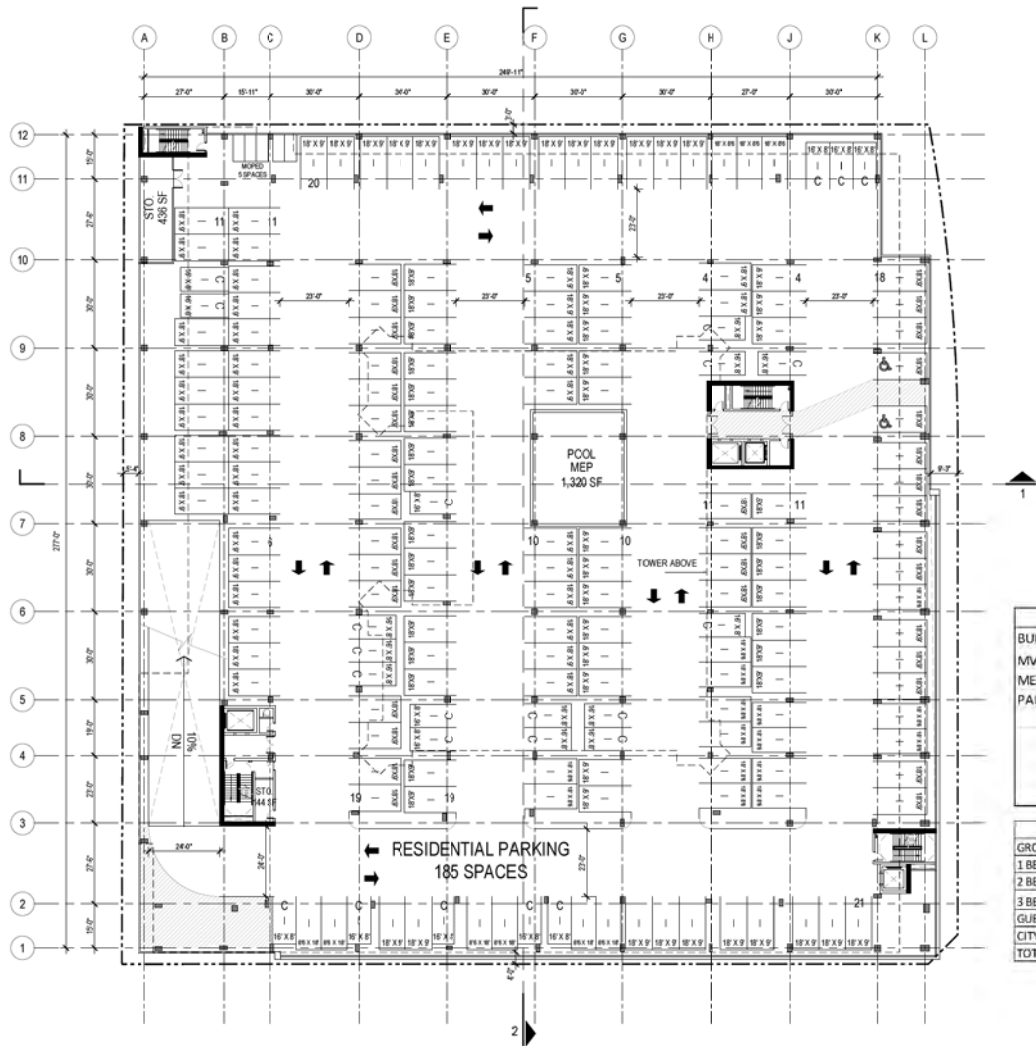
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SHEET CONTAINS:
LEVEL 3 FLOOR PLAN
 #REV: 02

A414
 HP4 15.257





LEVEL 4

BUILDING AREA:	72,843 SF
MVP:	1,097 SF
MEP/STORAGE:	1,842 SF
PARKING/SERVICES:	69,904 SF
TOTAL PARKING 185 SPACES	
STANDARD	148 SPACES
COMPACT	24 SPACES
ADA	2 SPACES
TANDEM	11 SPACES

	STANDARD	COMPACT	ADA	TANDEM	TOTAL
GROCERY	-	-	-	-	-
1 BED	134	20	1	-	155
2 BED	9	4	1	6	20
3 BED	5	-	-	5	10
GUEST	-	-	-	-	-
CITY	-	-	-	-	-
TOTAL	148	24	2	11	185



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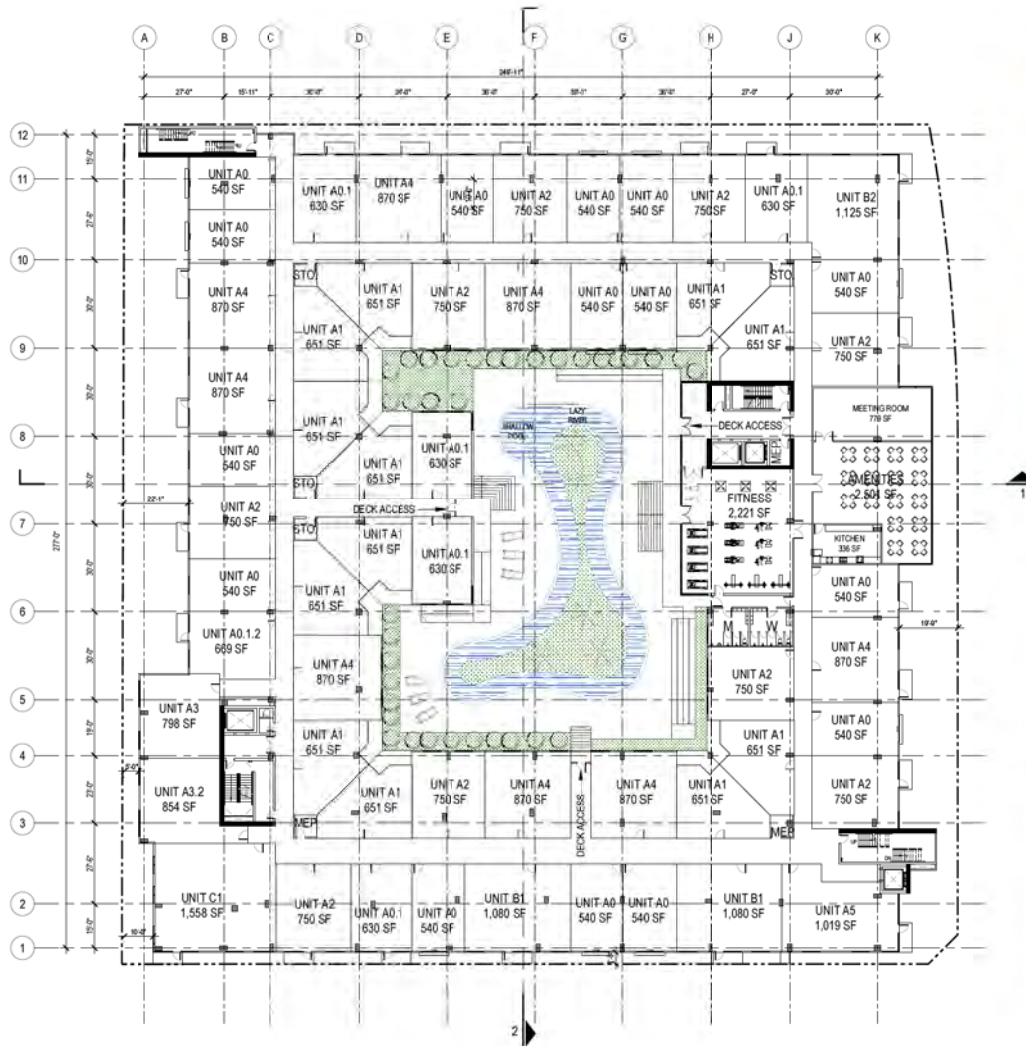
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PROJECT: HUP-18-0001
LEVEL 4 FLOOR PLAN
 DATE: 08/22/18

A415

HP4 15.257



POOL COURTYARD / AMENITIES
CHARACTER IMAGES
SEE LANDSCAPE PLANS

ROOM NAME	AREA (sqm)	OCCUPANT RATE	OCCUPANTS
Assembly (Unconcentrated) - Outdoor Pool Deck	4,770	11	118
Assembly (Unconcentrated) - Amenities	2,860	11	107
Pool	1,920	10	28
Fitness	1,271	10	64
Residential	40,957	200	344

Total occupants: 642
IRC 2021

LEVEL 5	
BUILDING AREA	55,507 SF
MVP	1,347 SF
FITNESS/AMENITIES	4,722 SF
MEP/STORAGE	566 SF
GROSS RESIDENTIAL AREA	48,872 SF
CIRCULATION	7,917 SF
NET RESIDENTIAL AREA	40,955 SF
EFFICIENCY:	83.8%
UNIT COUNT:	57 53A, 2B, 1C



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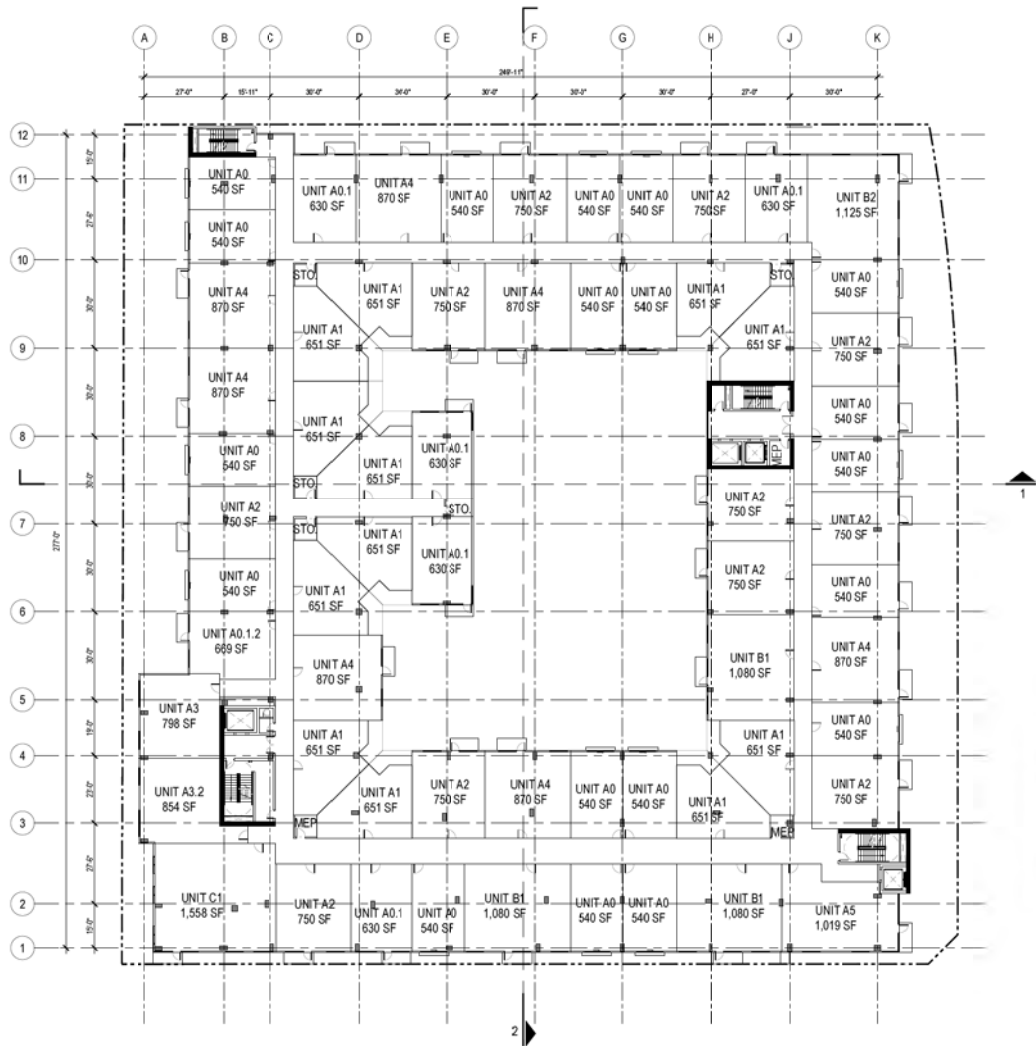
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SHEET CONTAINS
LEVEL 5 FLOOR PLAN
SHEET NO.

A416
HP4 15.257



LEVELS 6-7 (2 LEVELS)	
BUILDING AREA:	53,879 SF
MVP:	1,097 SF
MEP/ STORAGE:	566 SF
GROSS RESIDENTIAL AREA:	52,216 SF
CIRCULATION:	7,391 SF
NET RESIDENTIAL AREA:	44,825 SF
EFFICIENCY:	85.8%
UNIT COUNT:	63 58A, 4B, 1C

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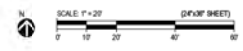
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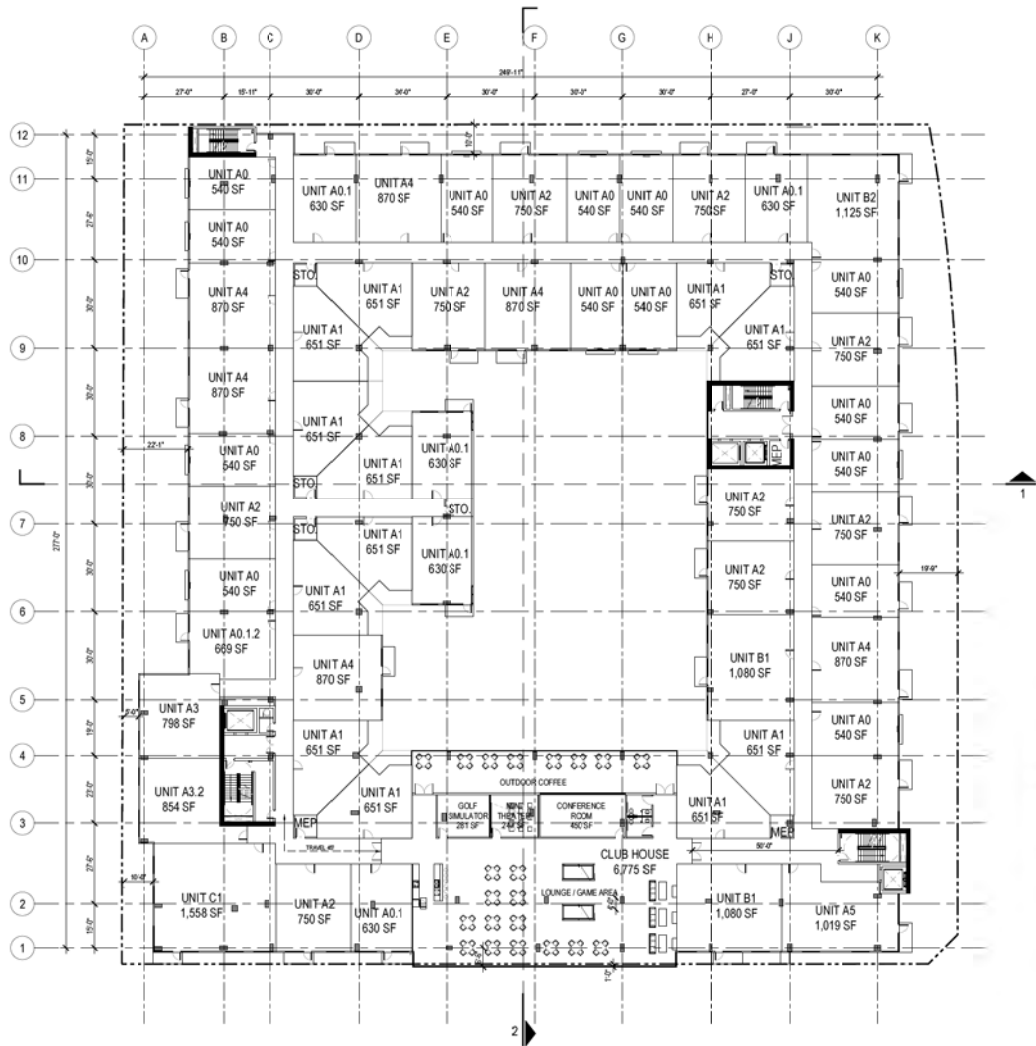
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SHEET CONTAINS
LEVELS 6-7
FLOOR PLAN

6/20/16
A417

HRA 15267





LEVEL 8	
BUILDING AREA:	54,329 SF
MVP:	1,097 SF
MEP/STORAGE:	566 SF
CLUB HOUSE:	6,837 SF
GROSS RESIDENTIAL AREA:	45,829 SF
CIRCULATION:	6,404 SF
NET RESIDENTIAL AREA:	39,425 SF
EFFICIENCY:	86.0%
UNIT COUNT:	55 51A, 3B, 1C

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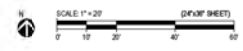
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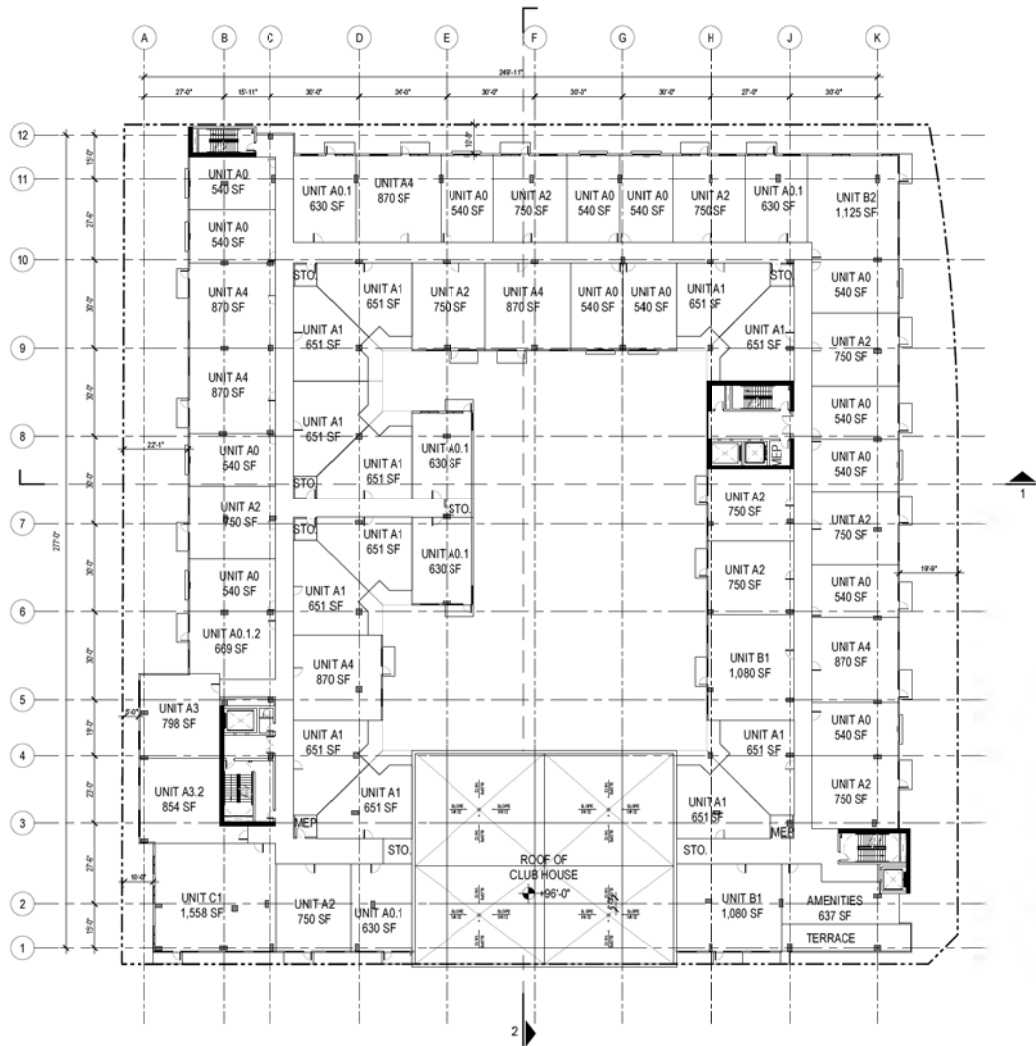
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SHEET CONTROLS
LEVEL 8 FLOOR PLAN
 SHEET NO.

A418
 HP4 15.257





LEVEL 9	
BUILDING AREA:	47,316 SF
MVP:	1,097 SF
AMENITIES:	637 SF
MEP/ STORAGE:	566 SF
GROSS RESIDENTIAL AREA:	45,016 SF
CIRCULATION:	6,610 SF
NET RESIDENTIAL AREA:	38,406 SF
EFFICIENCY:	85.3%
UNIT COUNT:	54 50A, 3B, 1C

NO.	DATE	REVISION

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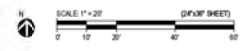
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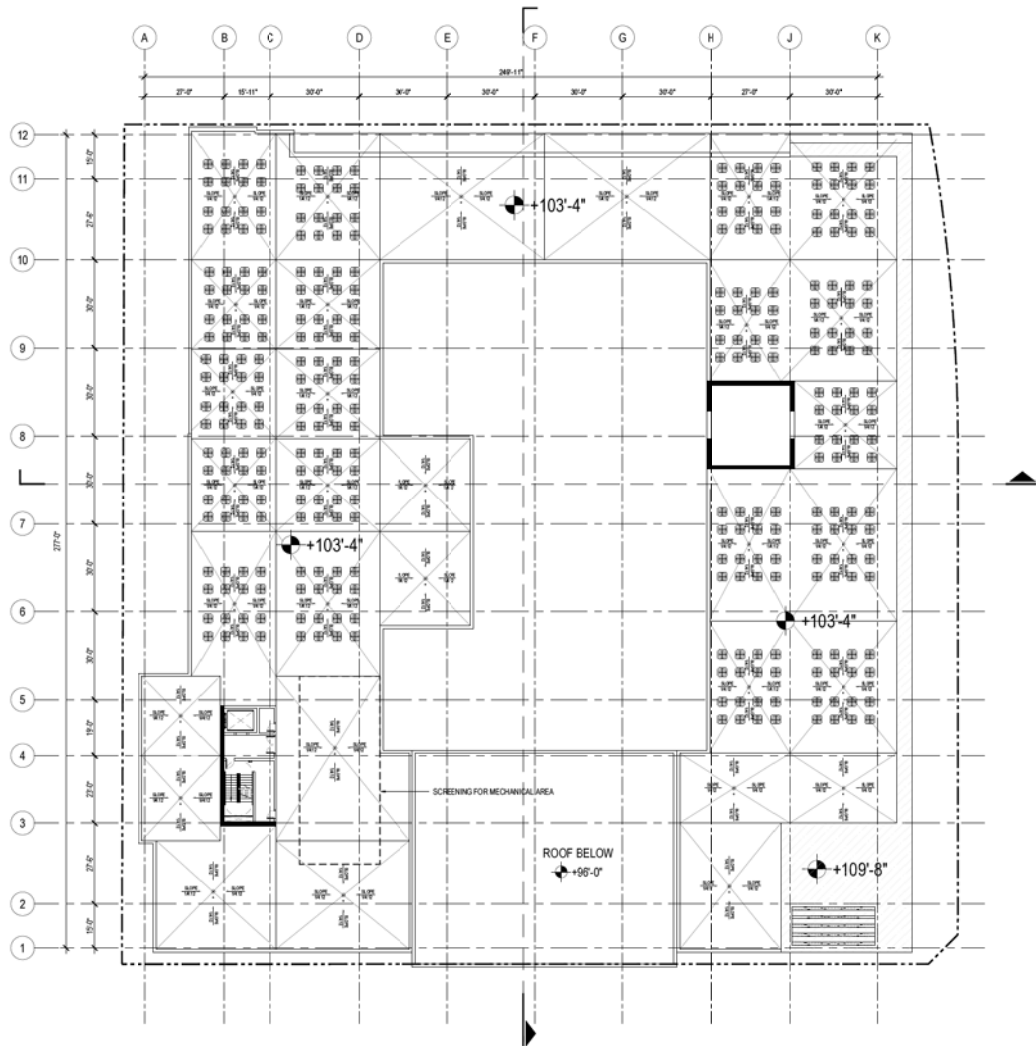
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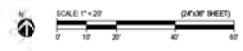
SHEET CONTAINS:
LEVEL 9 FLOOR PLAN

A419
 HP4 15.257





ROOF PLAN



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SHEET COMPRISED
ROOF PLAN

SHEET NO.
A420

HP4 15.257



UNIT A0
540 SF
STUDIO
81 UNITS



UNIT A0.1
630 SF
JR. 1 BED
25 UNITS



UNIT A0.1.2
665 SF
JR. 1 BED
5 UNITS



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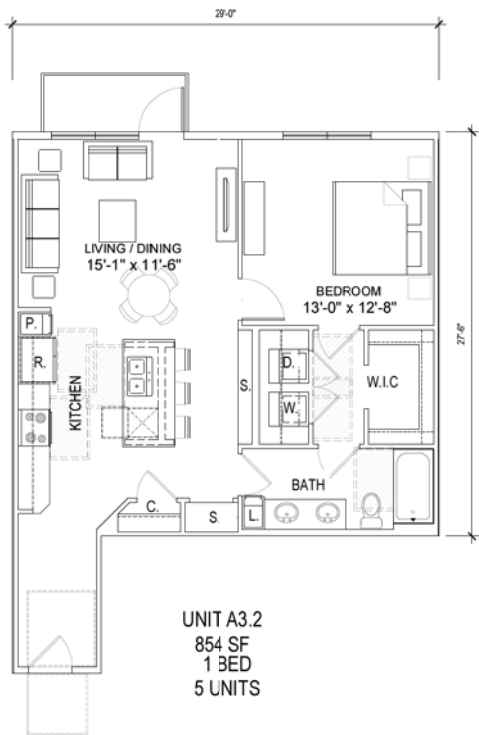
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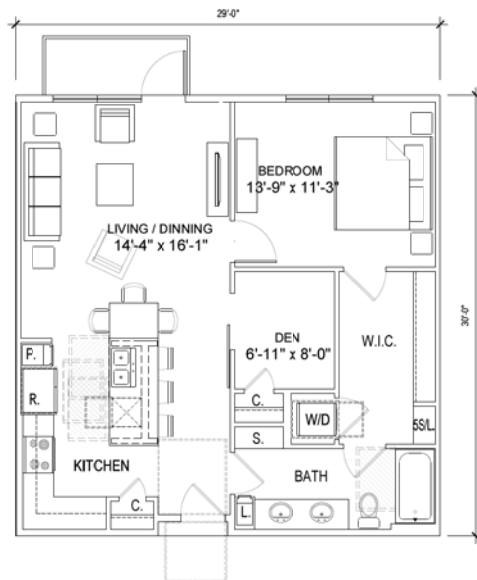
SHEET CONTAINS
UNIT PLANS
SHEET NO.

A301
HPA 152207

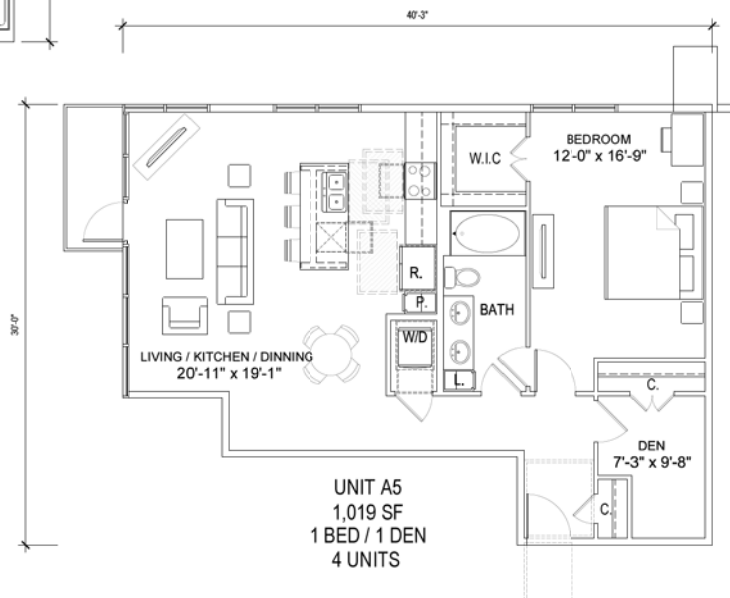
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UNIT A3.2
854 SF
1 BED
5 UNITS



UNIT A4
870 SF
1 BED / 1 DEN
34 UNITS



UNIT A5
1,019 SF
1 BED / 1 DEN
4 UNITS



CITY PLANNING SUBMITTAL - 05.05.2016

SHEET NUMBER
UNIT
PLANS

682740

A302

HP& 15257

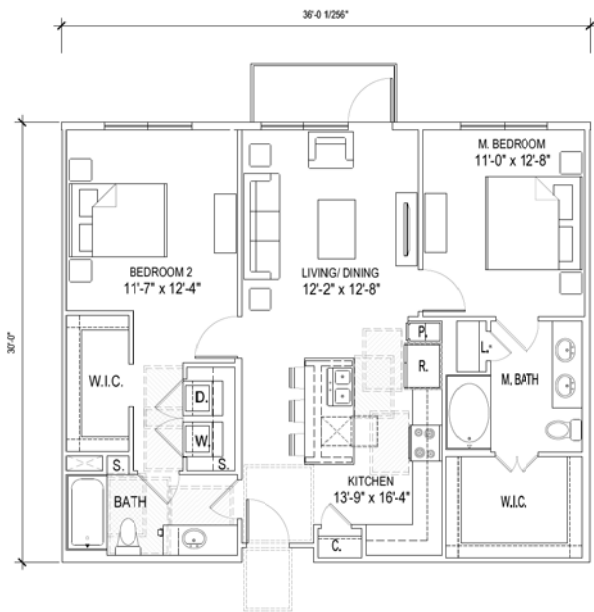
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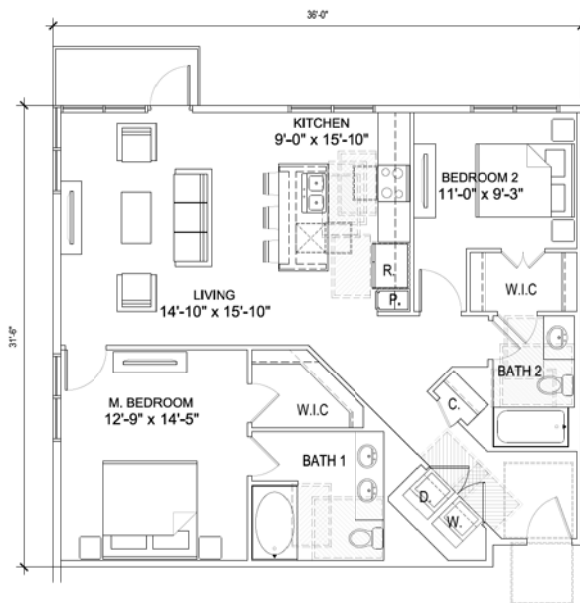
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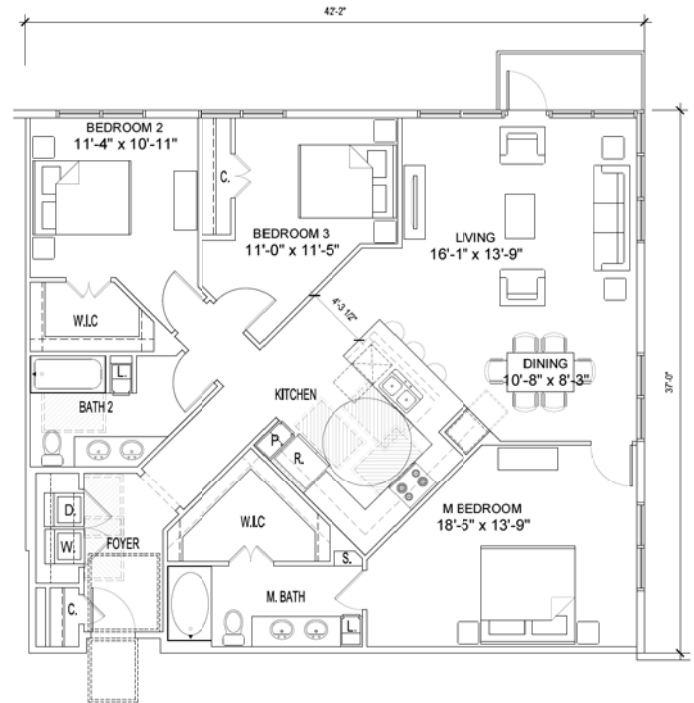
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UNIT B1
1,080 SF
2 BED / 2 BATH
12 UNITS



UNIT B2
1,125 SF
2 BED / 2 BATH
5 UNITS



UNIT C1
1,558 SF
3 BED / 2 BATH
5 UNITS

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SHEET NUMBER
UNIT PLANS

682740

A302

HP& 15257



CITY PLANNING SUBMITTAL - 05.05.2016

REVISIONS		
#	DATE	COMMENTS

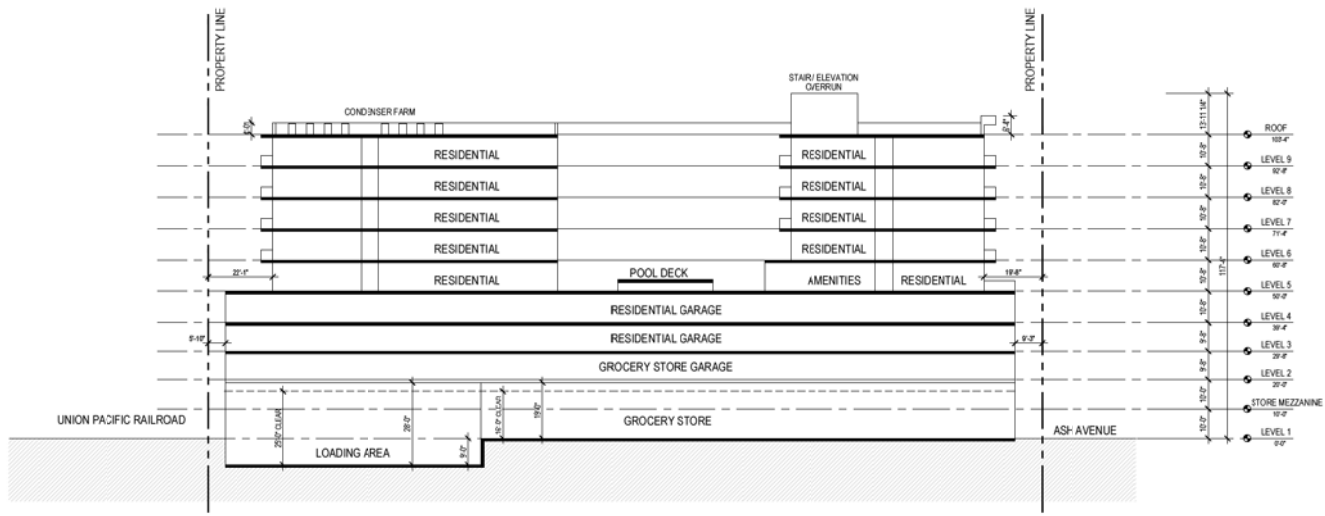
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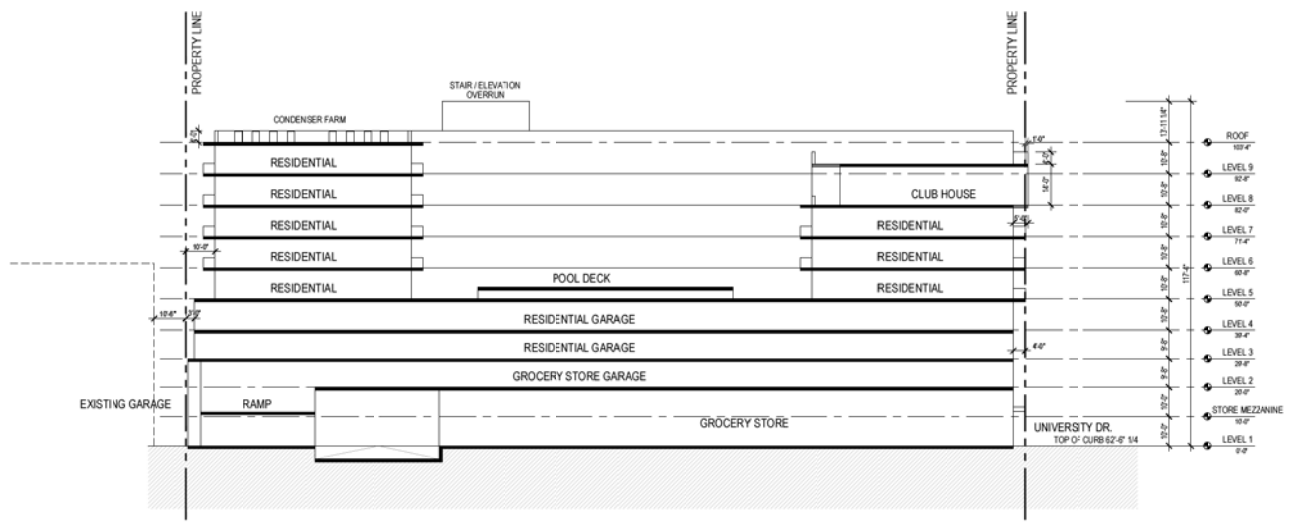
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NEWPORT BEACH-SCOTTSDALE-TORONTO-VALENTIA-VIENNA-WASHINGTON

SHEET CONTAINS
SECTIONS
6/8/27 (4)
A501
HP& 15257

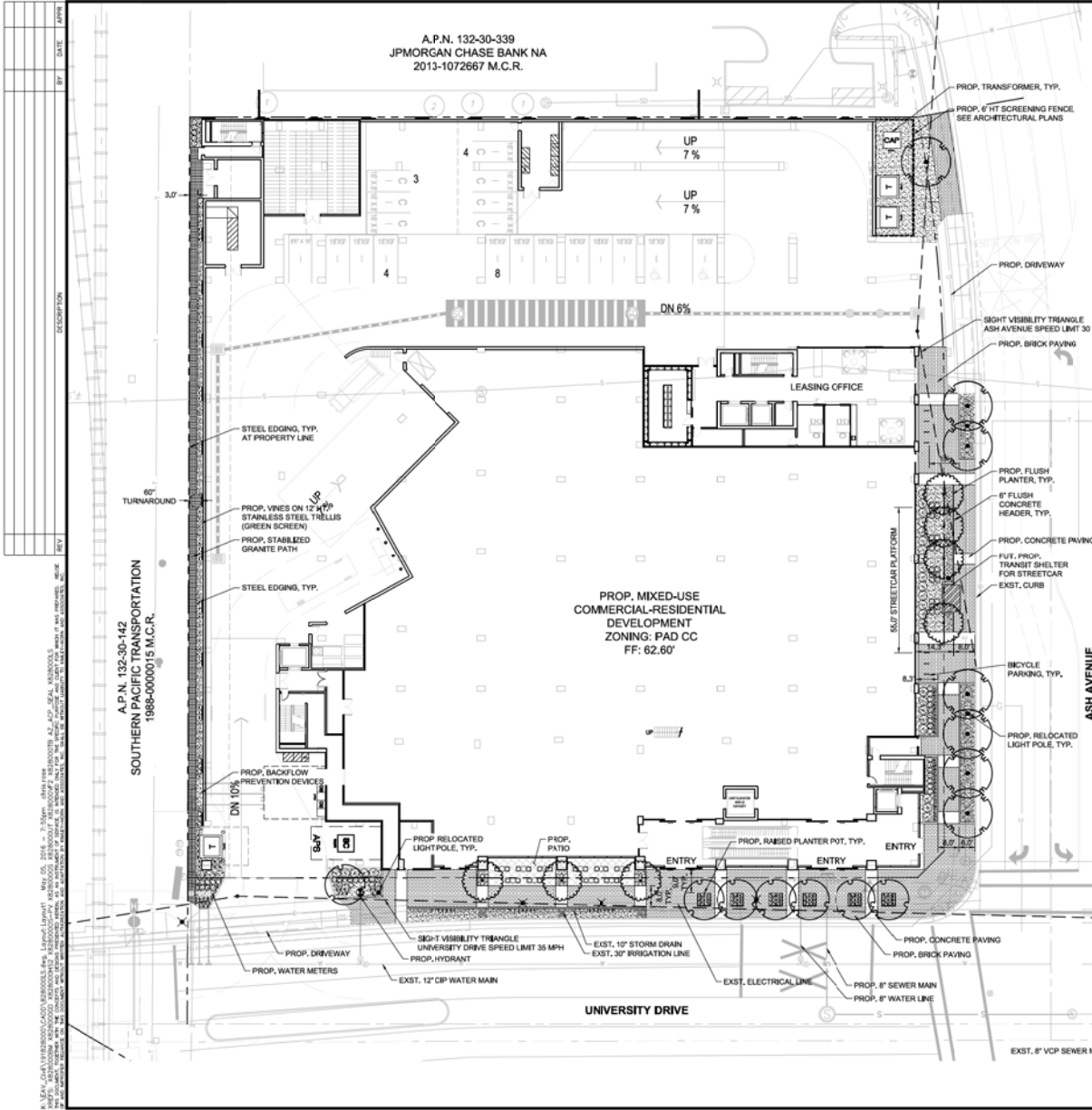


SECTION 1



SECTION 2





CITY OF TEMPE REQUIREMENTS

CODE SEC.	REQUIRED	PROVIDED
4-702	Min. 1-1/2" caliper trees	Provided
4-703	(1) Street tree per 30 LF street frontage University Drive (296 LF) = 10 Trees Ash Avenue (276 LF) = 10 Trees	10 Trees 11 Trees

REQUIRED	PROVIDED
Ash Avenue - Minimum 70% Brick Paving	75% Provided
University Drive - Minimum 30% Brick Paving	84% Provided



PLANTING LEGEND

TREES

BOTANICAL NAME / COMMON NAME	PLANTING SIZE	QTY
Acacia aneura Mulga	1-1/2" Caliper, min. 6" ht. Full crown, specimen Single Trunk	7
Laurus nobilis 'Saratoga' Saratoga Bay Laurel (fruitless variety)	1-1/2" Caliper, min. 6" ht. Full crown, specimen Single Trunk, upright	7
Ulmus parvifolia Evergreen Elm	1-1/2" Caliper, min. 6" ht. Full crown, specimen Single Trunk	7

SHRUBS & GROUNDCOVERS

BOTANICAL NAME / COMMON NAME	PLANTING SIZE	QTY
Agave desmetiana Smooth Agave	5 Gal, 12" ht.	51
Bougainvillea spp Bougainvillea	5 Gal.	25
Convolvulus creorum Bush Morning Glory	1 Gal.	148
Hesperaloe parviflora Red Yucca	5 Gal.	15
Macfadynena unguis-cati Cattail Vine	5 Gal.	34
Muhlenbergia rigida 'Nashville' Purple Muttly	5 Gal. 4" x 6"	73
Nandina domestica Heavenly Bamboo	5 Gal.	23
Pedilanthus macrocarpus Lady Slipper	5 Gal, 12" ht.	24
Plumbago scandens 'Summer Snow' White Plumbago	5 Gal.	20
Ruellia brittoniana Common Ruellia	5 Gal.	31
Ruellia brittoniana 'Katie' Dwarf Blue Ruellia	1 Gal.	128
Teucrium chamaedrys Germander	5 Gal.	51
Zephyranthes x 'Prairie Sunset' Coral Lily	1 Gal.	24

INERT MATERIALS / PAVING

DESCRIPTION / SPECIFICATION	QTY
Decomposed Granite: 1" Screened, 2" depth; Color: 'Express Brown' by Granite Express or approved equal	4,015 SF
Patio Specialty Paving: Integral Colored Concrete, light broom finish Davis Colors '160 Canyon'	340 SF
Sidewalk Brick Paving: Brick: 'Tempe Antigua Blend' by Phoenix Brick Yard; install per City of Tempe Standard Details	5,265 SF
Stabilized Granite Path: 3/8" Minus, 3" depth with 1/4" x 5" Steel Edging; Color: 'Express Brown' by Granite Express or approved equal	905 SF

SITE AMENITIES

DESCRIPTION / SPECIFICATION	QTY
Raised Planter Pot Quick-Crete QR-CAL6036P, 60" SQ, x 36" ht, square planter pot Manufacturer's Std. Color: 'Lattice' with 'Strata' Finish, or approved equal	6
Relocated Light Pole	7
Bike Rack 'Emerson' Bike Rack by Landscape Forms, or approved equal 14 bike spaces provided	7



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1-800-STRA-11
OFFICIAL NUMBER SERVICE

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UNION, ARIZONA 85002 (480) 207-2666

NEC UNIVERSITY AND ASH
PRELIMINARY LANDSCAPE CONCEPT
TEMPE, ARIZONA

PROJECT No.
191526209
SCALE (H): 1"=30'
SCALE (V): NONE
DRAWN BY: CMR
DESIGN BY: CMR
CHECK BY: ACP
DATE: 05/06/16
NOT FOR CONSTRUCTION
SUNOLS.dwg
L1
1 OF 2 SHEETS

PRELIMINARY CONCEPT SUBJECT TO CHANGE



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 1865 W. Chandler Road, Suite 200
 Mesa, Arizona 85207 (480) 207-2666

NEC UNIVERSITY AND ASH
 PRELIMINARY LANDSCAPE CONCEPT
 POOL COURTYARD
 TEMPE, ARIZONA

PROJECT No.
19162600
 SCALE (H): 1"=30'
 SCALE (V): NONE
 DRAWN BY: CMR
 DESIGN BY: CMR
 CHECK BY: ACP
 DATE: 05/06/16
 LANDSCAPE ARCHITECT
 ANTHONY M. POPE
 04258
 CHRISTOPHER M. ROSE
 03111
 NOT FOR CONSTRUCTION
 05/06/16 - 04/04/17
 L2
 2 OF 2 SHEETS

PLANTING LEGEND

TREES

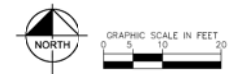
- | | |
|--|---|
| | BOTANICAL NAME / COMMON NAME
Chamaerops humilis
Mediterranean Fan Palm |
| | Sophora secundiflora
Texas Mountain Laurel |

SHRUBS & GROUNDCOVERS

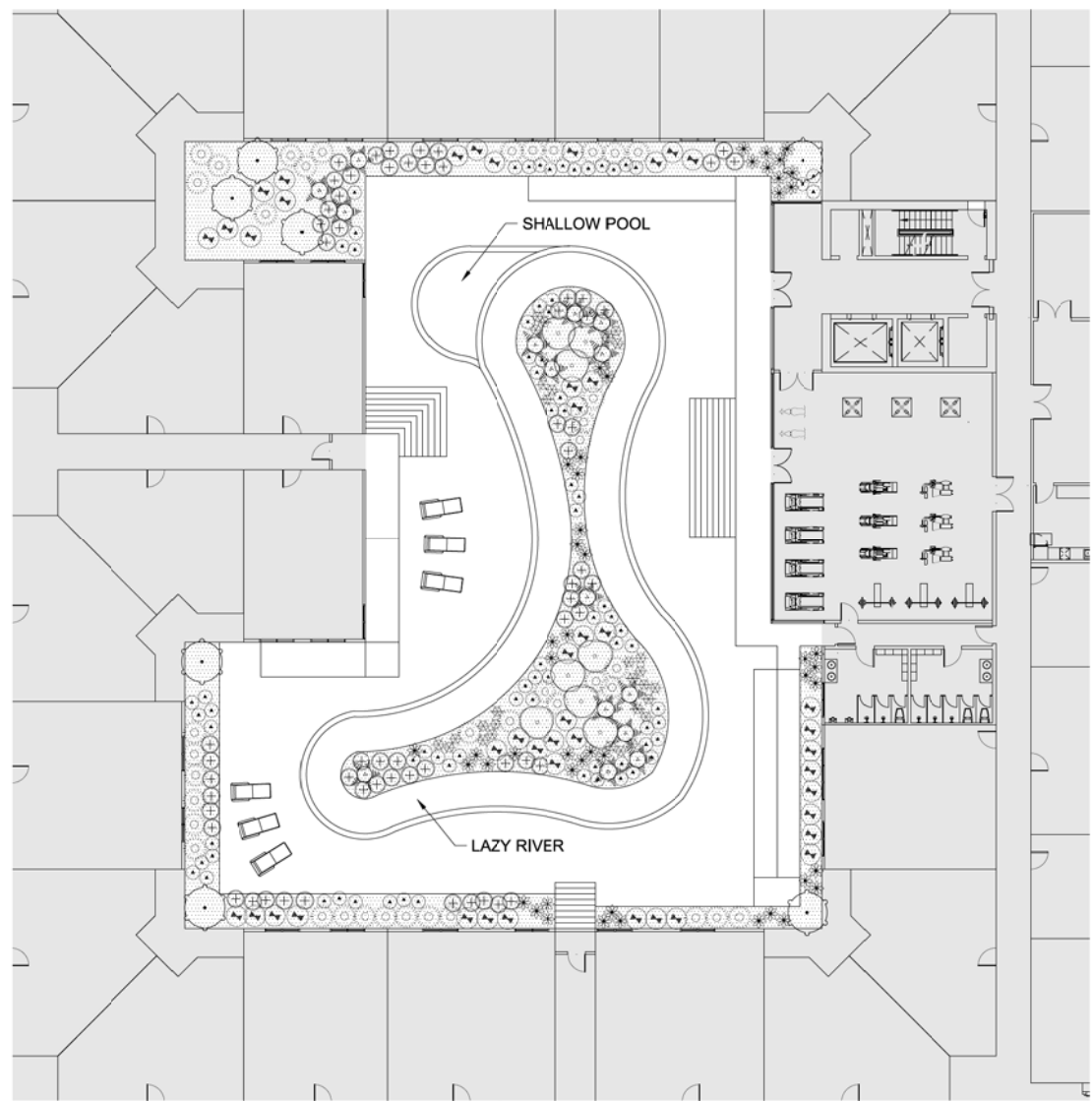
- | | |
|--|---|
| | BOTANICAL NAME / COMMON NAME
Agave desmetiana
Smooth Agave |
| | Agave geminiflora
Twin Flower Agave |
| | Sabalina frutescens
Yellow Bulbine |
| | Cassia gilliesii
Mexican Bird of Paradise |
| | Justicia candicans
Red Justicia |
| | Muhlenbergia c. 'Regal Mist'
Regal Mist Muhly |
| | Wedelia trilobata
Yellow Dot |

INERT MATERIALS / PAVING

- | | |
|--|---|
| | DESCRIPTION / SPECIFICATION
Decomposed Granite: 1/4" Minus,
2" depth; Color: TBD |
|--|---|



NO.	DATE	DESCRIPTION





MATERIALS / COLOR KEY NOTES

- A. GARAGE SCREENING
PERFORATED METAL PANEL 51% OPENINGS
MORIN SURREY BEIGE & DOVE GRAY
- B. STUCCO FINISH - 3 COAT SYSTEM
GRAY SCREEN
SHERWIN WILLIAMS SW 7011
- C. STUCCO FINISH - 3 COAT SYSTEM
WEB GRAY
SHERWIN WILLIAMS SW 7075
- D. MASONRY
SUMMIT
VICTORIAN
- E. METAL PANEL
MORIN
BLUE GRAY
- F. VINYL WINDOW
FIBREX DARK BRONZE
CLEAR/ DUAL PANEL / LOW-E
- G. ALUMINUM STOREFRONT WINDOW
EFCO SERIES 403
DARK BRONZE ANODIZED
- H. RAILING
METAL
POWDER COATED BLACK FINISH
- J. STUCCO FINISH - 3 COAT SYSTEM
RED BAY
SHERWIN WILLIAMS SW 6321
- K. STUCCO FINISH - 3 COAT SYSTEM
ACCESSIBLE BEIGE
SHERWIN WILLIAMS SW 7036
- L. METAL PANEL
MORIN
SPARTAN BRONZE
- M. WINDOW WALL
VIRACON
COLOR: SILVER, VFE 1-38
- N. PRECAST PANEL
TO BE BID
FINISH COLOR ACCESSIBLE BEIGE SW 7036
LIGHT SANDELAST
- O. PRECAST PANEL
TO BE BID
FINISH COLOR SAME AS "D" ABOVE



ISSUE DATE: 05/06/2016

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#	DATE	COMMENTS

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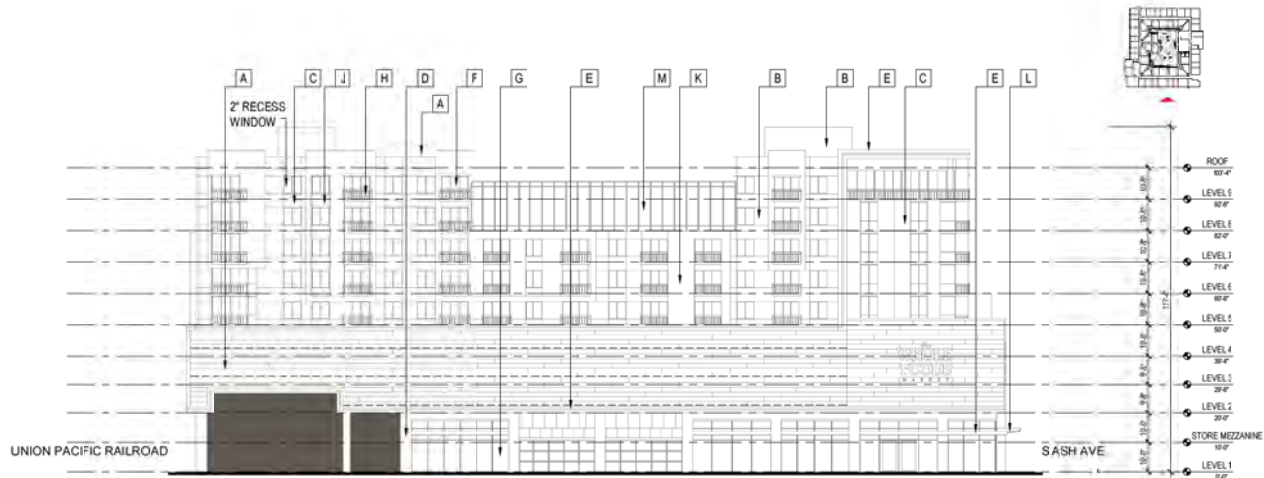
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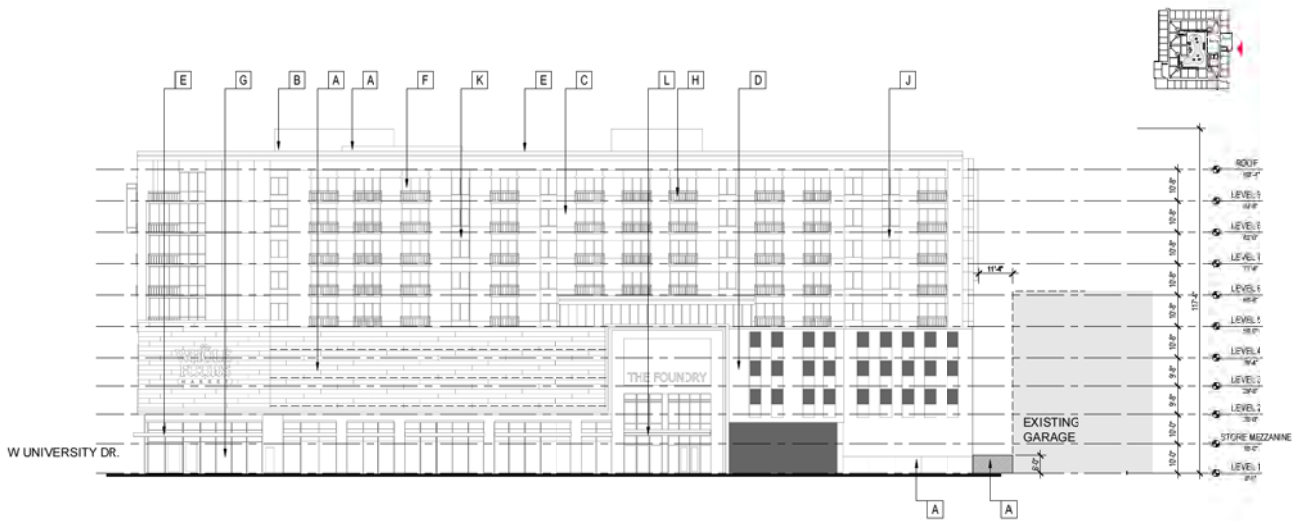
SHEET COMPLETE
**PERSPECTIVE
OPTION 1**

SHEET NO.
A510

HPA 15237



**SIDE STREET (SOUTH) ELEVATION
FACING UNIVERSITY DRIVE**



**FRONT (EAST) ELEVATION
FACING ASH AVENUE**

MATERIALS / COLOR KEY NOTES

- A. GARAGE SCREENING
PERFORATED METAL PANEL 51% OPENINGS
MORIN SURREY BEIGE & DOVE GRAY
- B. STUCCO FINISH - 3 COAT SYSTEM
GRAY SCREEN
SHERWIN WILLIAMS SW 7011
- C. STUCCO FINISH - 3 COAT SYSTEM
WEB GRAY
SHERWIN WILLIAMS SW 7075
- D. MASONRY
SUMMIT
VICTORIAN
- E. METAL PANEL
MORIN
BLUE GRAY
- F. VINYL WINDOW
FIBREX DARK BRONZE
CLEAR/ DUAL PANEL / LOW-E
- G. ALUMINUM STOREFRONT WINDOW
EFCO SERIES 403
DARK BRONZE ANODIZED
- H. RAILING
METAL
POWDER COATED BLACK FINISH
- J. STUCCO FINISH - 3 COAT SYSTEM
RED BAY
SHERWIN WILLIAMS SW 6321
- K. STUCCO FINISH - 3 COAT SYSTEM
ACCESSIBLE BEIGE
SHERWIN WILLIAMS SW 7036
- L. METAL PANEL
MORIN
SPARTAN BRONZE
- M. WINDOW WALL
VIRACON
COLOR: SILVER, VFE 1-38
- N. PRECAST PANEL
TO BE BID
FINISH COLOR ACCESSIBLE BEIGE SW 7036
LIGHT SANDBLAST
- O. PRECAST PANEL
TO BE BID
FINISH COLOR SAME AS "D" ABOVE



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SHEET COMMENTS
ELEVATIONS

A515

HP& 15257

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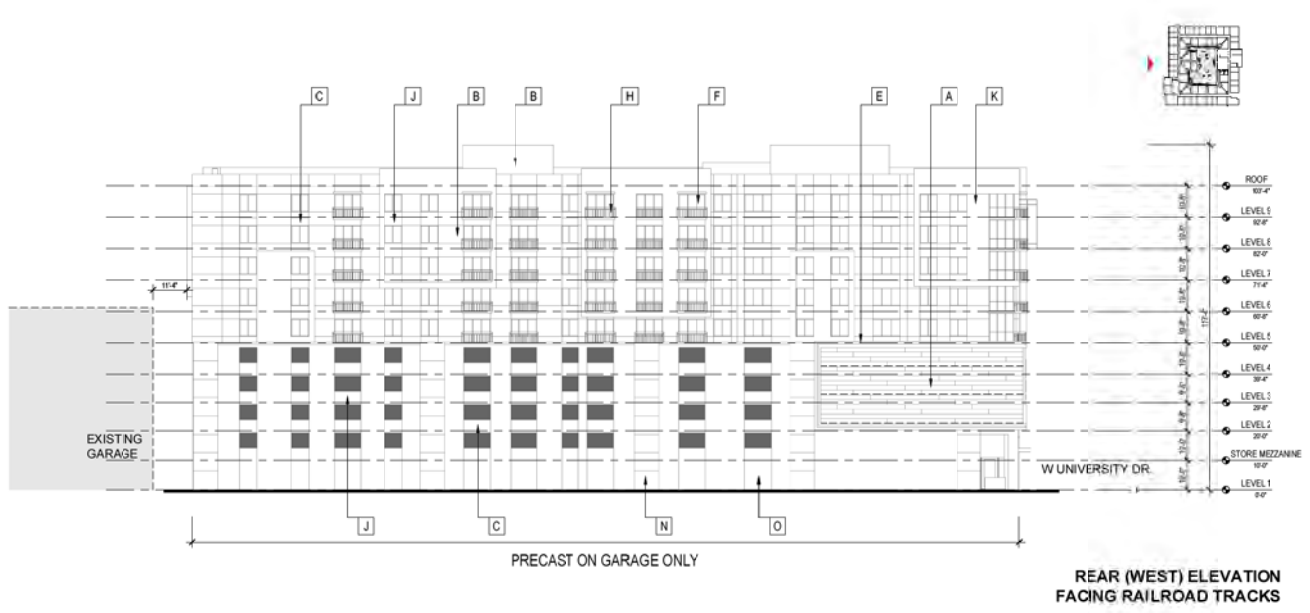
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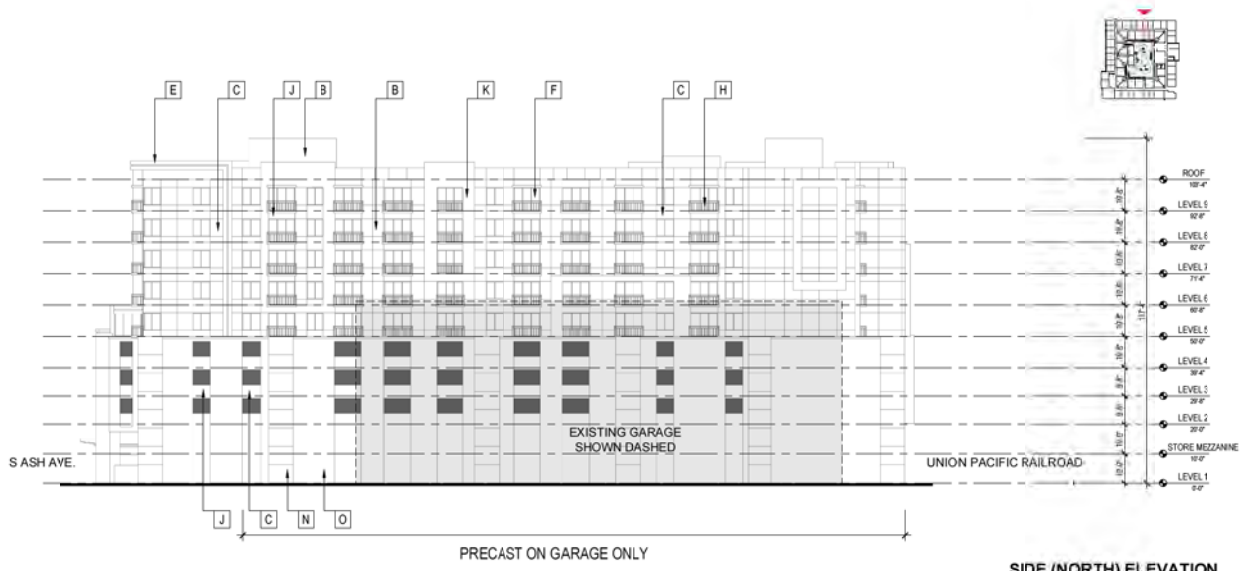
SHEET COMMENTS
ELEVATIONS

SHEET NO.

A516
HP& 15257



REAR (WEST) ELEVATION
FACING RAILROAD TRACKS

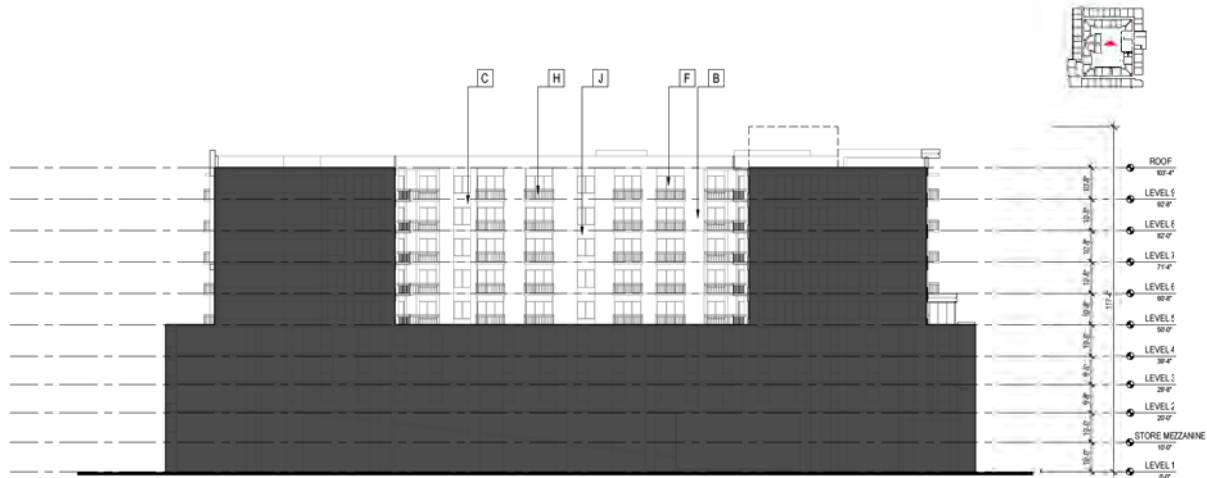


SIDE (NORTH) ELEVATION
FACING EXISTING PARKING STRUCTURE

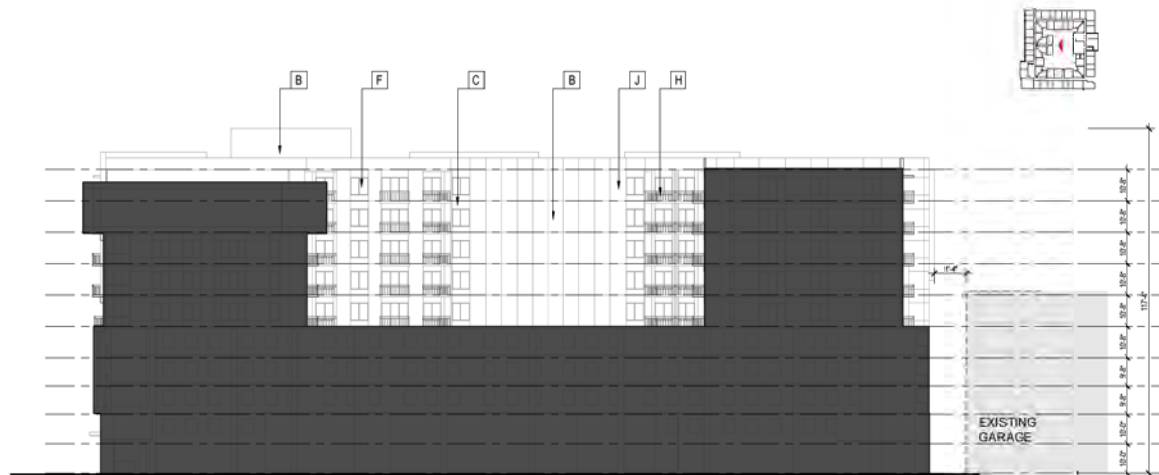
MATERIALS / COLOR KEY NOTES

- A. GARAGE SCREENING
PERFORATED METAL PANEL 51% OPENINGS
MORIN SURREY BEIGE & DOVE GRAY
- B. STUCCO FINISH - 3 COAT SYSTEM
GRAY SCREEN
SHERWIN WILLIAMS SW 7011
- C. STUCCO FINISH - 3 COAT SYSTEM
WEB GRAY
SHERWIN WILLIAMS SW 7075
- D. MASONRY
SUMMIT
VICTORIAN
- E. METAL PANEL
MORIN
BLUE GRAY
- F. VINYL WINDOW
FIBREX DARK BRONZE
CLEAR/ DUAL PANEL / LOW-E
- G. ALUMINUM STOREFRONT WINDOW
EFCO SERIES 403
DARK BRONZE ANODIZED
- H. RAILING
METAL
POWDER COATED BLACK FINISH
- J. STUCCO FINISH - 3 COAT SYSTEM
RED BAY
SHERWIN WILLIAMS SW 6321
- K. STUCCO FINISH - 3 COAT SYSTEM
ACCESSIBLE BEIGE
SHERWIN WILLIAMS SW 7036
- L. METAL PANEL
MORIN
SPARTAN BRONZE
- M. WINDOW WALL
VIRACON
COLOR: SILVER, VFE 1-38
- N. PRECAST PANEL
TO BE BID
FINISH COLOR ACCESSIBLE BEIGE SW 7036
LIGHT SANDBLAST
- O. PRECAST PANEL
TO BE BID
FINISH COLOR SAME AS "D" ABOVE





COURTYARD ELEVATION



COURTYARD ELEVATION

MATERIALS / COLOR KEY NOTES

A.	GARAGE SCREENING PERFORATED METAL PANEL 51% OPENINGS MORIN SURREY BEIGE & DOVE GRAY
B.	STUCCO FINISH - 3 COAT SYSTEM GRAY SCREEN SHERWIN WILLIAMS SW 7011
C.	STUCCO FINISH - 3 COAT SYSTEM WEB GRAY SHERWIN WILLIAMS SW 7075
D.	MASONRY SUMMIT VICTORIAN
E.	METAL PANEL MORIN BLUE GRAY
F.	VINYL WINDOW FIBREX DARK BRONZE CLEAR/ DUAL PANEL / LOW-E
G.	ALLUMINIUM STOREFRONT WINDOW EFCO SERIES 403 DARK BRONZE ANODIZED
H.	RAILING METAL POWDER COATED BLACK FINISH
J.	STUCCO FINISH - 3 COAT SYSTEM RED BAY SHERWIN WILLIAMS SW 6321
K.	STUCCO FINISH - 3 COAT SYSTEM ACCESSIBLE BEIGE SHERWIN WILLIAMS SW 7036
L.	METAL PANEL MORIN SPARTAN BRONZE
M.	WINDOW WALL VIRACON COLOR: SILVER, VFE 1-38
N.	PRECAST PANEL TO BE BID FINISH COLOR ACCESSIBLE BEIGE SW 7036 LIGHT SANDBLAST
O.	PRECAST PANEL TO BE BID FINISH COLOR SAME AS "D" ABOVE



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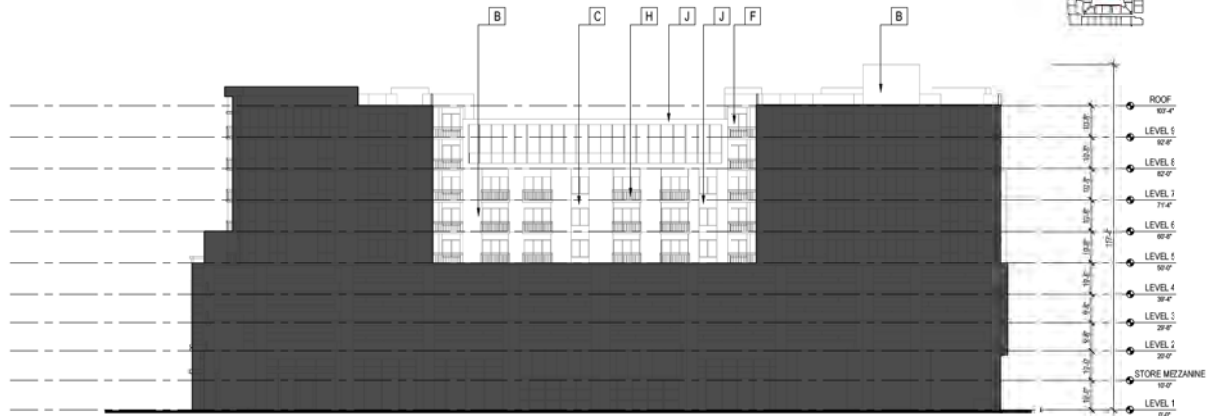
SHEET COMPLETE
ELEVATIONS

SHEET NO.

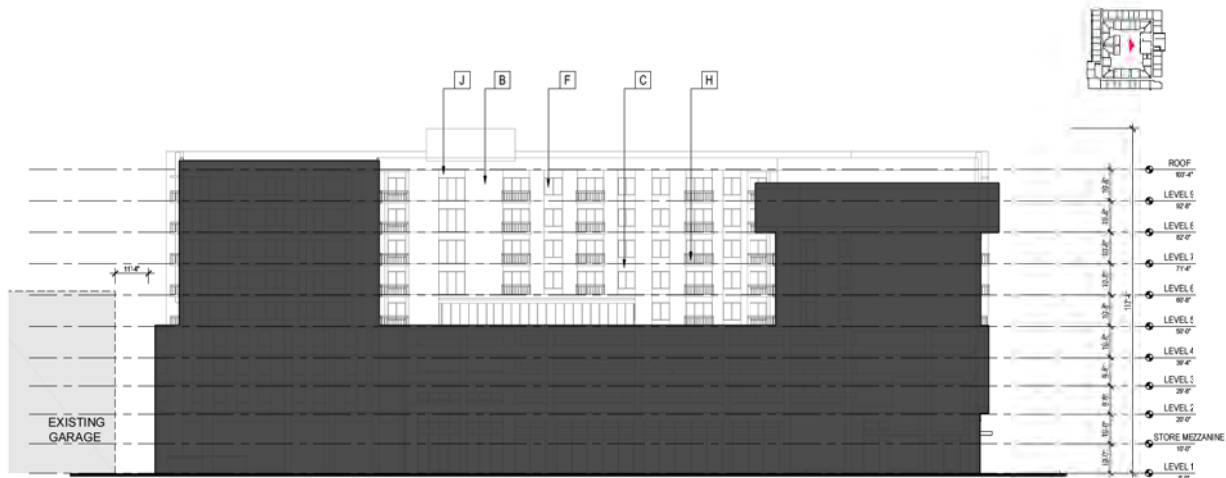
A517

HPA 15257

CITY PLANNING SUBMITTAL - 05.06.2016



COURTYARD ELEVATION



COURTYARD ELEVATION

MATERIALS / COLOR KEY NOTES

A.	GARAGE SCREENING PERFORATED METAL PANEL 51% OPENINGS MORIN SURREY BEIGE & DOVE GRAY
B.	STUCCO FINISH - 3 COAT SYSTEM GRAY SCREEN SHERWIN WILLIAMS SW 7011
C.	STUCCO FINISH - 3 COAT SYSTEM WEB GRAY SHERWIN WILLIAMS SW 7075
D.	MASONRY SUMMIT VICTORIAN
E.	METAL PANEL MORIN BLUE GRAY
F.	VINYL WINDOW FIBREX DARK BRONZE CLEAR/ DUAL PANEL / LOW-E
G.	ALUMINUM STOREFRONT WINDOW EFCO SERIES 403 DARK BRONZE ANODIZED
H.	RAILING METAL POWDER COATED BLACK FINISH
J.	STUCCO FINISH - 3 COAT SYSTEM RED BAY SHERWIN WILLIAMS SW 6321
K.	STUCCO FINISH - 3 COAT SYSTEM ACCESSIBLE BEIGE SHERWIN WILLIAMS SW 7036
L.	METAL PANEL MORIN SPARTAN BRONZE
M.	WINDOW WALL VIRACON COLOR: SILVER, VFE 1-38
N.	PRECAST PANEL TO BE BID FINISH COLOR ACCESSIBLE BEIGE SW 7036 LIGHT SANDBLAST
O.	PRECAST PANEL TO BE BID FINISH COLOR SAME AS "D" ABOVE



ISSUE DATE: 05/06/2016

REVISION	DATE	COMMENTS

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MEMPHIS-BEAVERCREEK-OTTAWA-TRONTON-CHICAGO-INDIANAPOLIS-MONTEVIDEO

SHEET COMMENTS
ELEVATIONS

SHEET NO.

A518

HP& 15257



**SIDE STREET (SOUTH) ELEVATION
FACING UNIVERSITY DRIVE**



**FRONT (EAST) ELEVATION
FACING ASH AVENUE**

MATERIALS / COLOR KEY NOTES

- A. GARAGE SCREENING
PERFORATED METAL PANEL 51% OPENINGS
MORIN SURREY BEIGE & DOVE GRAY
- B. STUCCO FINISH - 3 COAT SYSTEM
GRAY SCREEN
SHERWIN WILLIAMS SW 7011
- C. STUCCO FINISH - 3 COAT SYSTEM
WEB GRAY
SHERWIN WILLIAMS SW 7075
- D. MASONRY
SUMMIT
VICTORIAN
- E. METAL PANEL
MORIN
BLUE GRAY
- F. VINYL WINDOW
FIBREX DARK BRONZE
CLEAR/ DUAL PANEL / LOW-E
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DARK BRONZE ANODIZED
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ACCESSIBLE BEIGE
SHERWIN WILLIAMS SW 7036
- L. METAL PANEL
MORIN
SPARTAN BRONZE
- M. WINDOW WALL
VIRACON
COLOR: SILVER, VFE 1-38
- N. PRECAST PANEL
TO BE BID
FINISH COLOR ACCESSIBLE BEIGE SW 7036
LIGHT SANDBLAST
- O. PRECAST PANEL
TO BE BID
FINISH COLOR SAME AS "D" ABOVE



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NEWPORT BEACH-SOFTWOODALE-THIRTTYFOURTH-EMERALD-COAST-PORTLAND

SHEET COMPLETION
ELEVATIONS

SHEET NO.
A511

HP& 15257



REAR (WEST) ELEVATION
FACING RAILROAD TRACKS



SIDE (NORTH) ELEVATION
FACING EXISTING PARKING STRUCTURE

MATERIALS / COLOR KEY NOTES

A.	GARAGE SCREENING PERFORATED METAL PANEL 51% OPENINGS MORIN SURREY BEIGE & DOVE GRAY
B.	STUCCO FINISH - 3 COAT SYSTEM GRAY SCREEN SHERWIN WILLIAMS SW 7011
C.	STUCCO FINISH - 3 COAT SYSTEM WEB GRAY SHERWIN WILLIAMS SW 7075
D.	MASONRY SUMMIT VICTORIAN
E.	METAL PANEL MORIN BLUE GRAY
F.	VINYL WINDOW FIBREX DARK BRONZE CLEAR/ DUAL PANEL / LOW-E
G.	ALLUMINIUM STOREFRONT WINDOW EFCO SERIES 403 DARK BRONZE ANODIZED
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O.	PRECAST PANEL TO BE BID FINISH COLOR SAME AS "D" ABOVE



REVISIONS

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THE FOUNDRY
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FORUM REAL ESTATE GROUP

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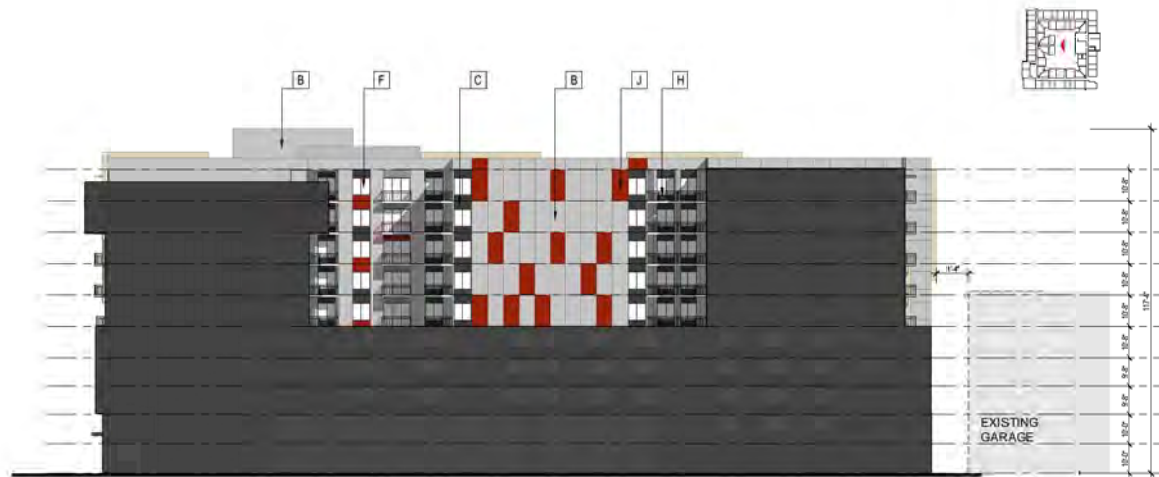
SHEET COMPLETION
ELEVATIONS

A512

HP& 15257



COURTYARD ELEVATION



COURTYARD ELEVATION

MATERIALS / COLOR KEY NOTES

A.	GARAGE SCREENING PERFORATED METAL PANEL 51% OPENINGS MORIN SURREY BEIGE & DOVE GRAY
B.	STUCCO FINISH - 3 COAT SYSTEM GRAY SCREEN SHERWIN WILLIAMS SW 7011
C.	STUCCO FINISH - 3 COAT SYSTEM WEB GRAY SHERWIN WILLIAMS SW 7075
D.	MASONRY SUMMIT VICTORIAN
E.	METAL PANEL MORIN BLUE GRAY
F.	VINYL WINDOW FIBREX DARK BRONZE CLEAR/ DUAL PANEL / LOW-E
G.	ALLUMINIUM STOREFRONT WINDOW EFCO SERIES 403 DARK BRONZE ANODIZED
H.	RAILING METAL POWDER COATED BLACK FINISH
J.	STUCCO FINISH - 3 COAT SYSTEM RED BAY SHERWIN WILLIAMS SW 6321
K.	STUCCO FINISH - 3 COAT SYSTEM ACCESSIBLE BEIGE SHERWIN WILLIAMS SW 7036
L.	METAL PANEL MORIN SPARTAN BRONZE
M.	WINDOW WALL VIRACON COLOR: SILVER, VFE 1-38
N.	PRECAST PANEL TO BE BID FINISH COLOR ACCESSIBLE BEIGE SW 7036 LIGHT SANDBLAST
O.	PRECAST PANEL TO BE BID FINISH COLOR SAME AS "D" ABOVE

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SHEET COMPLETE
ELEVATIONS
SHEET #10

A513
HPA 15257

CITY PLANNING SUBMITTAL - 05.06.2016





REVISIONS

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NEWPORT BEACH BOULDER COLORADO SPRINGS DENVER HOUSTON LOS ANGELES

SHEET COMMENTS
PERSPECTIVE VIEW 1

SHEET NO.
A105

CITY PLANNING SUBMITTAL: 05.05.2016

HPA 15.257



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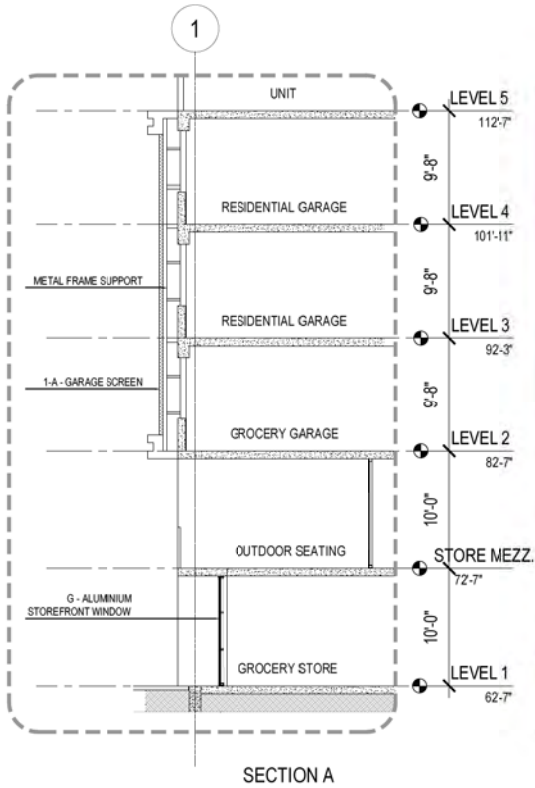
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NEWPORT BEACH BOULDER DENVER PHOENIX PORTLAND SEATTLE

SHEET COMMENT: **PERPECTIVE VIEW 2**

SHEET NO: **A106**

CITY PLANNING SUBMITTAL: 05/05/2015

HPA 15267



W. UNIVERSITY DR. PERSPECTIVE

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SHEET COMMENTS
PERSPECTIVE STORE MEZZ. BALC.

A107

HP4 15.257



S. ASH AVE. PERSPECTIVE

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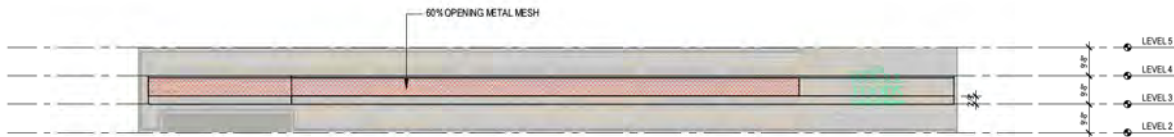
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SHEET COMMENTS
PERSPECTIVE
RESIDENTIAL ENTRY

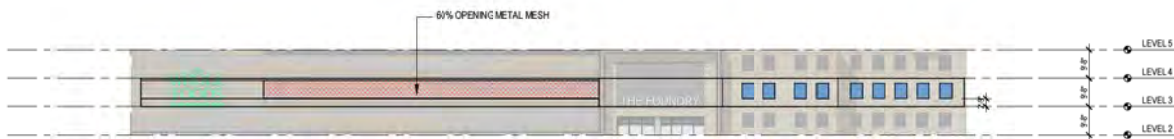
A108

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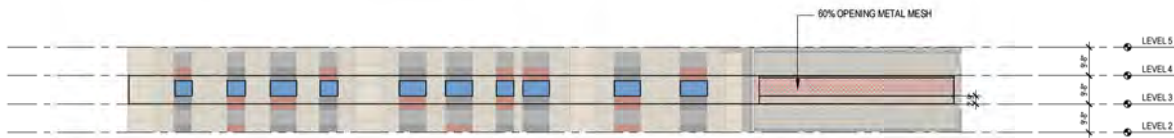
CITY PLANNING SUBMITTAL - 05.05.2016



SOUTH ELEVATION	TOTAL ELEVATION AREA PER LEVEL	100% Open	51% Open
	AREA OF EXTERIOR WALL OPENINGS PER LEVEL	1,319 SF	698 SF
	% OF PROPOSED OPENINGS	51.6%	26.3%



EAST ELEVATION	TOTAL ELEVATION AREA PER LEVEL	100% Open	51% Open
	AREA OF EXTERIOR WALL OPENINGS PER LEVEL	891 SF	546 SF
	% OF PROPOSED OPENINGS	32.9%	20.1%



WEST ELEVATION	TOTAL ELEVATION AREA PER LEVEL	100% Open	51% Open
	AREA OF EXTERIOR WALL OPENINGS PER LEVEL	810 SF	611 SF
	% OF PROPOSED OPENINGS	29.3%	22.5%



NORTH ELEVATION	TOTAL ELEVATION AREA PER LEVEL	2508 SF
	AREA OF EXTERIOR WALL OPENINGS PER LEVEL	356 SF
	% OF PROPOSED OPENINGS	14.2%

TOTAL	TOTAL ELEVATION AREA PER LEVEL	100% Open	51% Open
	AREA OF EXTERIOR WALL OPENINGS PER LEVEL	3,426 SF	2,210 SF
	% OF PROPOSED OPENINGS	32.3%	20.9%

406.5.2 OPENINGS.

FOR NATURAL VENTILATION PURPOSES, THE EXTERIOR SIDE OF THE STRUCTURE SHALL HAVE UNIFORMLY DISTRIBUTED OPENINGS ON TWO OR MORE SIDES. THE AREA OF SUCH OPENINGS IN EXTERIOR WALLS ON A TIER SHALL BE NOT LESS THAN 20 PERCENT OF THE TOTAL PERIMETER WALL AREA OF EACH TIER. THE AGGREGATE LENGTH OF THE OPENINGS CONSIDERED TO BE PROVIDING NATURAL VENTILATION SHALL BE NOT LESS THAN 40 PERCENT OF THE PERIMETER OF THE TIER. INTERIOR WALLS SHALL BE NOT LESS THAN 20 PERCENT OPEN WITH UNIFORMLY DISTRIBUTED OPENINGS.

EXCEPTION: OPENINGS ARE NOT REQUIRED TO BE DISTRIBUTED OVER 40 PERCENT OF THE BUILDING PERIMETER WHERE THE REQUIRED OPENINGS ARE UNIFORMLY DISTRIBUTED OVER TWO OPPOSING SIDES OF THE BUILDING.



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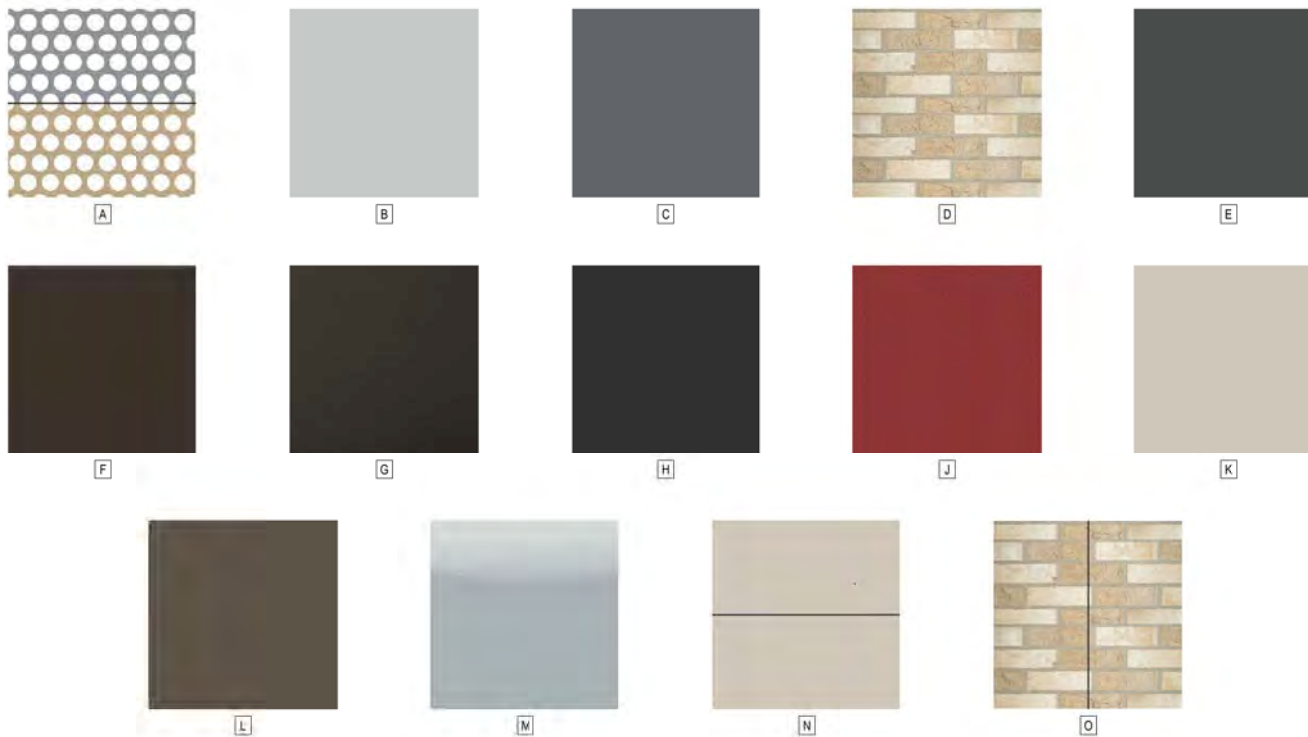
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SHEET CONTENTS
GARAGE OPENING CALCULATION
SHEET NO.

A520
HPA 15207

CITY PLANNING SUBMITTAL - 05.06.2016

MATERIAL SAMPLE BOARD



PRECAST PANEL WITH INSET BRICK REFERENCE IMAGES

THIN BRICK

Odcastle offers owners and architects the option to integrally-cast brick in concrete. From the nation's premier manufacturers of thin brick, there is a wide array of brick options available. Advantages of brick-embedded concrete include no flashing, lintels or weep cavities; no efflorescence; and reduced construction time.

The images shown here represent a small sampling of the available color and texture blends. Brick corners and edge caps are also available. The Odcastle representative will make available more accurate color literature and samples when requested.



MATERIALS / COLOR KEY NOTES

A.	GARAGE SCREENING PERFORATED METAL PANEL 51% OPENINGS MORIN SURREY BEIGE & DOVE GRAY
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D.	MASONRY SUMMIT VICTORIAN
E.	METAL PANEL MORIN BLUE GRAY
F.	VINYL WINDOW FIBREX DARK BRONZE CLEAR/ DUAL PANEL / LOW-E
G.	ALLUMINUM STOREFRONT WINDOW EFCO SERIES 403 DARK BRONZE ANODIZED
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SHEET CONTAINS
MATERIAL SAMPLE

SHEET NO.
A519

HP& 15237

Morin Color Samples - Metal Panels (Solid & Perforated)



Blue Gray



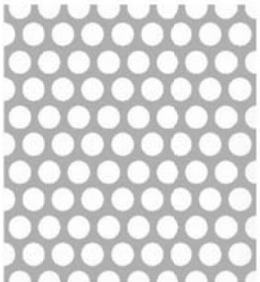
Dove Gray



Spartan Bronze



Surrey Beige



Dove Gray



Spartan Bronze



**THE FOUNDRY
PUBLIC INVOLVEMENT FINAL REPORT**

1. THE FOUNDRY - PUBLIC INVOLVEMENT FINAL REPORT (DS160108)

Lot 1, Cosmopolitan (MCR Bk 900 P 35) – NWC University Drive & Ash Avenue.

2. NOTIFICATION DATES

Pursuant to Section 6-401(F) of the Zoning and Development Code, all property owners within 600' and registered Community, Neighborhood, and Homeowner's Associations within ¼ mile of the Site were notified of the neighborhood meeting as follows:

- ◆ March 21, 2016: Neighborhood Meeting Notices sent via First Class US Mail
- ◆ March 18, 2016: Neighborhood Meeting and Public Hearing Notification Sign posted on Site
- ◆ May 3, 2016: Public Hearing Notification Sign updated with new hearing dates

3. NOTIFICATION AREA MAPS

The notification area included property owners within 600' of the Site and registered neighborhood and homeowners' associations within ¼ mile of the Site. (See attached Exhibit – Notification Area Maps.)

4. REGISTERED NEIGHBORHOOD AND HOMEOWNERS' ASSOCIATIONS

The notification area included two Affiliate Associations, five Homeowners Associations, and four Neighborhood Associations. (See attached Exhibit – Notification Area Maps and Exhibit – Notification Lists.)

5. NEIGHBORHOOD MEETING

A neighborhood meeting was held per the City of Tempe Zoning and Development Code §6-401(F) on November 4, 2015 from 6 PM to 7 PM at the Dennis J. Cahill Senior Center, 715 East Fifth Street.

The meeting format was an open-house style with project presentation boards arranged centrally in the meeting room with members of the Forum development team available to greet participants and discuss the project in an inviting and comfortable setting. The open house format allowed one-on-one interaction with the interested neighbors as well as small group discussions. The development team felt this format was appropriate to ensure that all neighbor voices had an equal opportunity to engage the development team in constructive discussion about the project.

6. OUTREACH AND ATTENDANCE NUMBERS

The 600' property owner notification area included 65 property owners, 2 Affiliate Associations, 4 Neighborhood Associations, and 5 Homeowner's Associations. In total, 80 letters were sent to property

owners and association contacts. Additionally, the association contacts were sent notification via email addresses provided by the city. (See attached Exhibit – Notification Lists.)

Attendance at the meeting was approximately 45 people, including 7 members of the Forum development team and Tempe Deputy Community Development Director - Planning, Ryan Levesque. Sign-in sheets were presented to everyone who attended the meeting. Thirty-two neighbors provided contact information on the sign-in sheets. A small number of neighbors in attendance did not wish to sign in. (See attached Exhibit – Neighborhood Meeting Sign In Sheets)

At the neighborhood meeting, neighbors were offered the opportunity to express their concerns on comment cards provided by the Forum development team. A number of neighbors provided their support, comments, questions, and concerns on the comment cards. (See attached Exhibit - Neighborhood Meeting Comment Cards.)

7. FEEDBACK

The neighborhood meeting was scheduled to begin at 6 PM and end at 7 PM. The Forum team was present beginning at 5:00 to greet any early arrivals, and stayed until shortly after 8 PM after the last of the neighbors left. Neighbors began arriving at 5:35 PM and the last neighbors left after 7:45 PM. Throughout the evening, all seven Forum team members greeted neighbors, presented the project, answered questions, and provided responses to input from the neighbors in attendance. In addition to general questions about the project's design, access, and location of the parking, the following is a list of questions, comments, and concerns expressed by members of the public in attendance at the neighborhood meeting, along with Forum's response:

- Great project – go for it!
- Elevation is good, ensure that west elevation facing railroad tracks maintains softened aesthetic value.
- Like the project – many good pedestrian elements and grocery store.
- Super project – this is what is needed for the area. Very impressive, looking forward to seeing this when it is complete.
- Perfect use for this vacant lot. Very excited about Whole Foods within walking distance of home.
- Potential graffiti on building walls and parking garage screening.
 - Forum has been working with city staff to ensure that design elements are in place to prevent graffiti, including landscaping, building materials and articulation, and lighting.
- Bicycle safety at University and Ash corner, including through traffic south on Ash into the residential neighborhood.
 - Forum will be working with the city to re-stripe Ash Avenue to accommodate the proposed Streetcar and will not change the restriction on southbound traffic to the neighborhood.
 - Traffic generated by the project will not pose an increased risk to bicycle safety at this intersection or along the Site's street frontages. The updated bike lanes along University Drive will remain in place.
- More public open space should be provided, such as gathering spaces on the sidewalks and access from the street to private amenity areas.

- Forum and Whole Foods are committed to providing as much active space along the street frontages as possible. The ability to add more open public space is limited due to the size of the Site, the floor area necessary to accommodate a full service grocer, public utilities, and site design constraints such as ingress/egress, landscaping, and the proposed Streetcar stop.
- Forum and Whole Foods would like to continue discussions with city staff to encroach the outdoor dining area further into the University right-of-way consistent with similar uses on Mill Avenue.
- Activate the pedestrian experience along University Drive.
 - University Drive is activated by the provision of outdoor dining, pedestrian access to Whole Foods, and a storefront glazing system that provides visual permeability into the retail base of the structure.
 - As stated above, Forum and Whole Foods are working with the city to make the outdoor dining area along University as large as possible while still complying with the Americans With Disabilities requirements.
- Why are trees along University proposed in planters instead of in the ground?
 - Utilities in the right-of-way prevent Forum from planting trees directly in the ground. Forum will utilize large planters to provide more root volume for the trees, supporting full, mature trees that will have a better chance of surviving and providing shade.
- Provide shade along University and Ash.
 - Tress and building elements, including awnings and other projections will provide shade along both street frontages for the comfort of pedestrians.
- How will pets (dogs) be accommodated regarding walking and relieving themselves?
 - The residential programming will include a pet area for residents.
- Whole Foods is too expensive for residents to shop at.
 - Whole Foods offers a number of affordable options, including their 365 line. Residents can shop affordably at Whole Foods by shopping the 365 line, purchasing bulk products (nuts, oats, grains etc.), buying seasonal fruits and vegetables, shopping sales, and planning ahead.
- Luxury apartments are too expensive and this project is gentrifying the neighborhood. Too many units.
 - The proposed apartments will be market rate and will help bring residents to the area to support Tempe's economic development.
 - This project does not displace any existing residential development or residents, the lot is currently vacant.
 - The proposed density conforms with the General Plan projected residential density for the Site.
- Happy to see a grocery store finally coming to downtown Tempe, in walking and biking distance from many homes and businesses.
- The proposed project is a great development that will be very beneficial to the community.
- Love all of it!

Overall, the project was very well-received, with a number of neighbors expressing their support of the proposed development. Certain neighbors that were opposed to the project did not wish to speak constructively with members of the development team.

8. DOCUMENTATION

- ◆ Neighborhood Notification Letter (See attached Exhibit – Neighborhood Meeting Notice.)
- ◆ Property Posting Affidavit and Photo (See attached Exhibit – Affidavit of Sign Posting.)
- ◆ Affidavit of Public Hearing Notification for Property Ownership List and Map (See Attached Exhibit – Signed Affidavit Form.)
- ◆ Neighborhood Meeting Sign-In Sheet (See attached Exhibit – Neighborhood Meeting Sign-In)
- ◆ Neighborhood Meeting Comment Cards (See attached Exhibit - Neighborhood Meeting Comment Cards.)
- ◆ Updated Property Posting Affidavit and Photo (See attached Exhibit – Affidavit of Updated Sign Posting)

9. ONGOING PUBLIC INVOLVEMENT

We will continue to utilize involvement techniques and communication approaches outlined in the Public Involvement Plan to ensure that any interested parties are engaged throughout the entitlement process. To date, only several neighbors who support the project have reached out to the development team.

The most current information about upcoming meetings and comment opportunities will be available via email (darin@senderlaw.com) or telephone (480-966-6735).

EXHIBIT - CONTEXT PLAN (SITE PHOTOS)



EXHIBIT - SITE PHOTOS



1. NWC Site - View North



2. NWC Site - View East



3. NWC Site - View South



4. NWC Site - View West

EXHIBIT - SITE PHOTOS



5. NEC Site - View North



6. NEC Site - View East



7. NEC Site - View South



8. NEC Site - View West

EXHIBIT - SITE PHOTOS



9. SEC Site - View North



10. SEC Site - View East



11. SEC Site - View South



12. SEC Site - View West

EXHIBIT - SITE PHOTOS



13. SWC Site - View North



14. SWC Site - View East



15. SWC Site - View South



16. SWC Site - View West